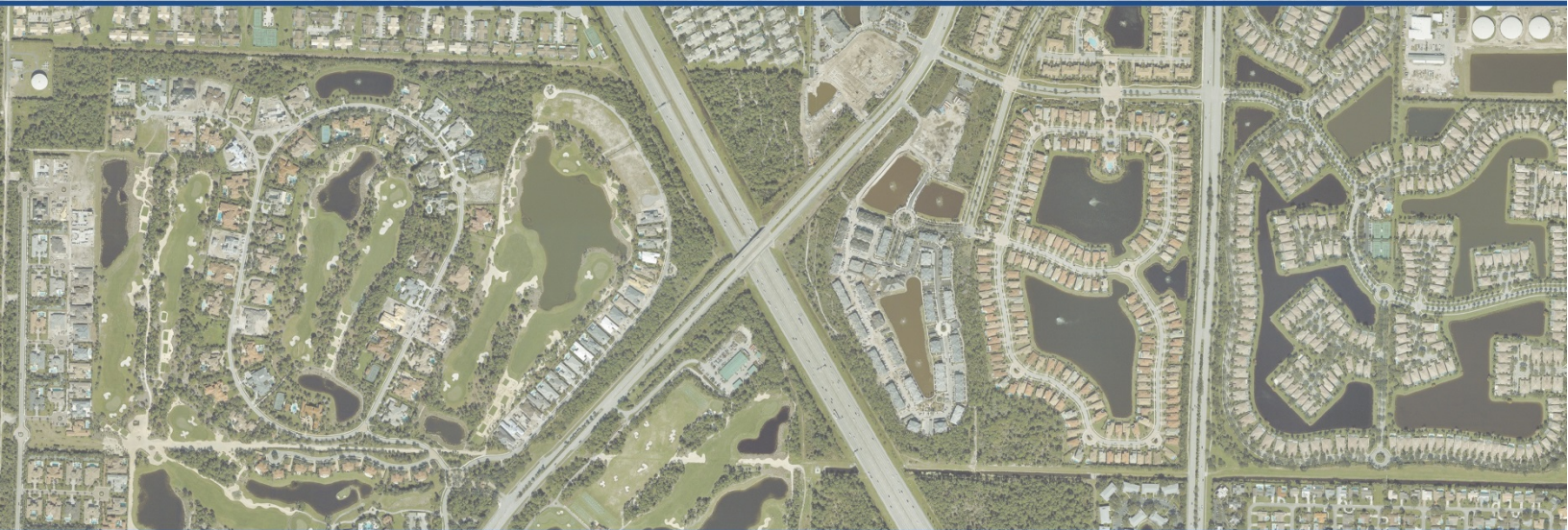


SOCIOCULTURAL EVALUATION REPORT

SR 9/1-95 at Central Boulevard Interchange **PD&E Study**

I-95 from north of PGA Boulevard (MP 36.783) to Donald Ross Road (MP 40.163)

ETDM 13748 • Palm Beach County • Financial Management Number: 413265-1-22-01 • Federal Aid Project No: N/A



Prepared for:
FDOT District 4
3400 West Commercial Blvd.
Fort Lauderdale, Florida 33309

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Prepared for:
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Fort Lauderdale, Florida 33309

October, 2016



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1.0 INTRODUCTION

The Florida Department of Transportation (FDOT) District Four is conducting a Project Development and Environment (PD&E) Study for the construction of a new interchange at I-95 and Central Boulevard in Palm Beach County, Florida. The limits of the study area extend along I-95 from north of PGA Boulevard (MP 36.783) to Donald Ross Road (MP 40.163), a distance of 3.38 miles.

The purpose of this report is to present the findings of the Sociocultural Effects (SCE) Evaluation for the proposed improvements. This SCE Report was prepared in accordance with the FDOT PD&E Manual, Part 2, Chapter 9 (Sociocultural Effects Evaluation, February 14, 2014) and the FDOT SCE Handbook (dated November 2005):

The SCE Evaluation is the process of determining and evaluating the effects a transportation action may have on a community and the quality of life of the citizens. SCE Evaluation is a proactive process that ensures that community values and concerns receive adequate attention during transportation development. The evaluation process is an integral part of project planning and development. The process focuses on a transportation projects' potential effects on social, economic, land use, mobility, aesthetic, and relocation issues. The SCE Evaluation process involves affected communities and residents, as well as transportation planners and decision makers, to evaluate the potential effects of a transportation action on a community and provides that human values and concerns receive due attention.

It is the policy of FDOT to work proactively with communities in implementing the principles, concepts and philosophy of Community Impact Assessment and SCE Evaluation throughout the transportation project development process. The collection and analysis of socio-cultural data allows the FDOT to fully understand the segments of the public that may be impacted by this project. Public participation was sought and encouraged throughout the study without regard to race, color, national origin, age, sex, religion, disability or family

2.0 PROJECT OVERVIEW

2.1 PROJECT BACKGROUND

The Florida Department of Transportation, District Four conducted an Interchange Justification Study to evaluate improvements to SR 9/I-95 that would reduce congestion and improve mobility in the northern Palm Beach County area, within the City of Palm Gardens. The limits of this study extended from north of Northlake Boulevard to south of Donald Ross Road, PGA Boulevard from west of Military Trail to west of Lake Victoria Gardens Drive; and Central Boulevard from 1.0 mile south of I-95 to 1.0 mile north of I-95. The limits of this study are shown in **Figure 1**.

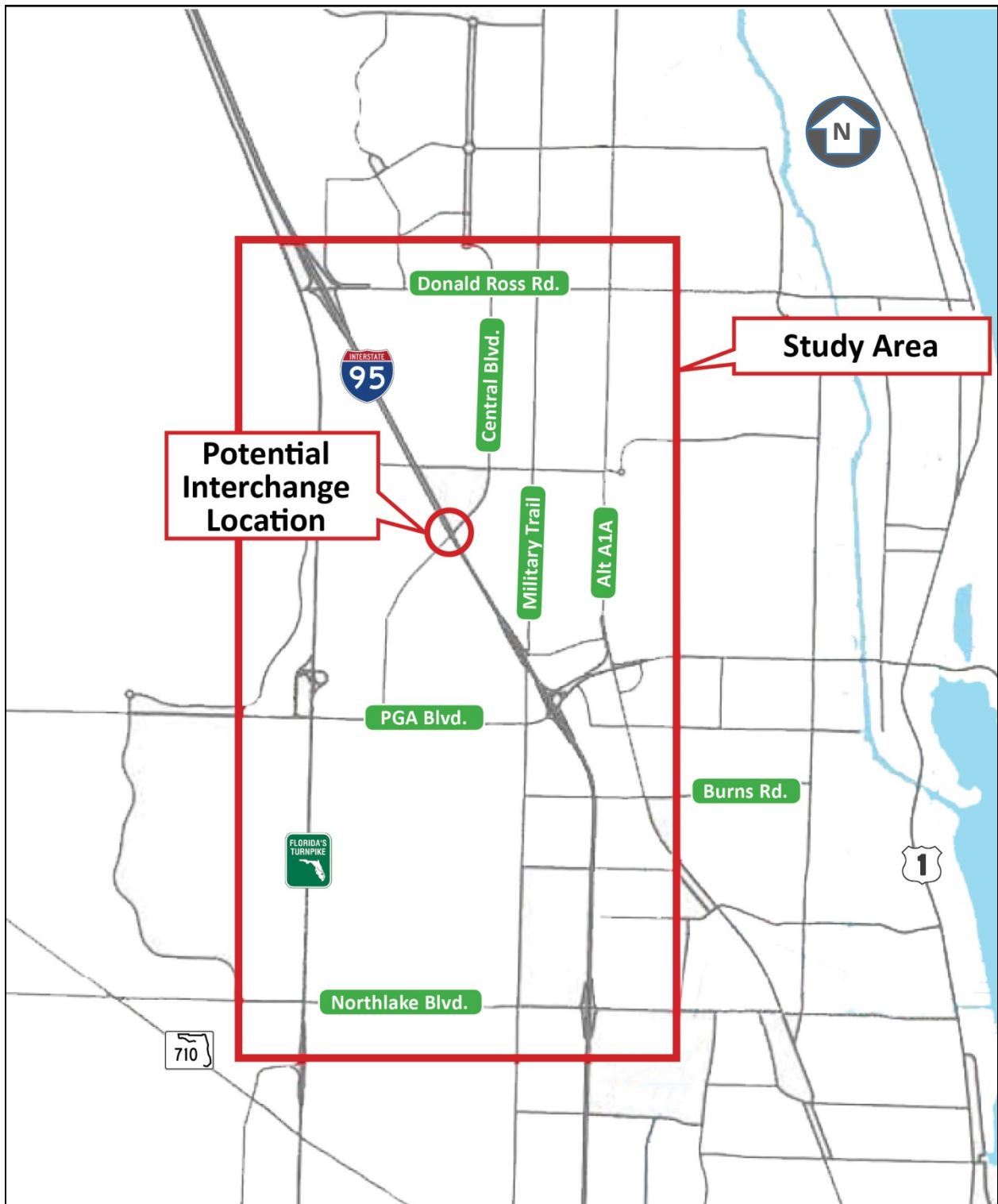


Specifically, this study focused on solutions that would reduce demand on regional transportation facilities, such as PGA Boulevard and Military Trail, by transferring that demand to other roadways with available capacity via a new or modified interchange between PGA Boulevard and Donald Ross Road along SR 9/I-95.

The Interchange Justification Report (IJR) was prepared in 2015. It concluded that a shift in demand to a new interchange at Central Boulevard would reduce the delay by approximately 1.4 million hours annually. The IJR was approved by the Federal Highway Administration (FHWA) in November, 2015. The Palm Beach County Metropolitan Planning Organization (MPO) 2040 Cost Feasible Plan was updated to include a new interchange at Central Boulevard. The Cost Feasible Plan was included in the MPO's Long Range Transportation Plan (LRTP), adopted in late 2014.

To address the improvements recommended in the IJR, FDOT initiated a Project Development and Environment (PD&E) study to evaluate potential improvements to SR 9/I-95 from north of PGA Boulevard (MP 36.783) to Donald Ross Road (MP 40.163), a distance of 3.38 miles. Specifically, the PD&E study evaluated alternatives for a new Interchange at Central Boulevard and for improvements to mainline I-95 within the reduced project limits.

Figure 1– IJR Study Area



2.2 PROJECT DESCRIPTION

The ongoing PD&E study is evaluating alternatives for construction of a new interchange at SR 9/I-95 and Central Boulevard in the City of Palm Beach Gardens in northern Palm Beach County. Construction of a new interchange, if selected over the No-Build Alternative as the Recommended Alternative, will reduce congestion and improve mobility within the City of Palm Beach Gardens. SR 9/I-95 is owned and operated by FDOT. It is classified in the Palm Beach County Comprehensive Plan as a Principal Arterial. Central Boulevard is classified as an Urban Collector. Central Boulevard currently crosses over, but does not provide access to, I-95 at this location.

The original study area identified for the IJR, and described for the PD&E study in the ETDM Project Summary Report, extended from Northlake Boulevard to the south to Donald Ross Road to the north, and from Florida's Turnpike to the west to Lake Victoria Gardens Boulevard to the east (**Figure 1**). However, since the IJR recommended construction of a new interchange at Central Boulevard to address congestion, the new limits of the PD&E Study were reduced to include the area influenced by the proposed improvements, as shown in **Figure 2**. The project limits for the PD&E study extend along I-95 from north of PGA Boulevard to Donald Ross Road. The proposed Central Boulevard interchange would be located approximately 1.0 mile north of the existing Military Trail (SR 809) partial interchange, and 2.0 miles south of the existing Donald Ross Road interchange.

2.3 PURPOSE AND NEED

The purpose of the project is to improve operational capacity and overall traffic operations by determining if a new interchange at Central Blvd at I-95 will relieve traffic congestion at the existing interchange of SR 9 (I-95) and SR 786 (PGA Boulevard). Conditions at PGA Boulevard are anticipated to deteriorate below acceptable level of service (LOS) standards if no improvements occur by 2035; the interchange will have insufficient capacity to accommodate the projected travel demand. The need for the project is based on the following primary and secondary criteria:

PRIMARY CRITERIA

CAPACITY/TRANSPORTATION DEMAND: Improve Operational Capacity and Overall Traffic Operations (Level of Service)

Proposed construction of a new interchange at I-95 and Central Boulevard is anticipated to improve traffic operations by reducing demand at the PGA Boulevard interchange and study area roadways and continue to meet the future travel demand projected as a result of Palm Beach County population and employment growth. According to traffic data presented in the I-95 Area Wide Mobility Study, the northbound I-95 ramp terminal intersection at PGA Boulevard is currently operating at LOS E/F (AM/PM Peak Hours) and the intersection of PGA Boulevard at Military Trail is currently operating at LOS E (AM/PM Peak Hours). By year

Figure 2– PD&E Study Limits



unacceptable conditions, including the southbound I-95 ramp terminal intersection at PGA Boulevard to LOS F (PM Peak Hour), the intersection of PGA Boulevard and Central Boulevard to LOS F (AM/PM Peak Hours) and the intersection of PGA Boulevard at Florida's Turnpike to LOS F (AM/PM Peak Hours). The existing and projected future traffic conditions for the study area roadways are as follows:

I-95 (South of PGA Boulevard)

-Existing Conditions-

2011 Annual Average Daily Traffic (AADT): 145,000

2011 Truck AADT: 6.4% (9,280 trucks per day)

LOS C (8 General Use and 2 HOV Lanes)

-Future Conditions-

2035 AADT: 182,400

2035 Truck AADT: 6.4% (11,674 trucks per day)

LOS D (8 General Use and 2 HOV Lanes)

PGA Boulevard (Florida's Turnpike to Military Trail)

-Existing Conditions-

2011 AADT: 42,000

2011 Truck AADT: 4.8% (2,016 trucks per day)

LOS D (6 Lanes)

-Future Conditions-

2035 AADT: 55,700

2035 Truck AADT: 4.8% (2,674 trucks per day)

LOS F (6 Lanes)

PGA Boulevard (Military Trail to I-95)

-Existing Conditions-

2011 AADT: 37,000

2011 Truck AADT: 7.0% (2,590 trucks per day)

LOS D (6 Lanes)

-Future Conditions-

2035 AADT: 69,200

2035 Truck AADT: 7.0% (4,844 trucks per day)

LOS F (6 Lanes)

PGA Boulevard (I-95 to Alt A1A)

-Existing Conditions-

2011 AADT: 64,500

2011 Truck AADT: 2.6% (1,677 trucks per day)

LOS F (6 General Use plus 1 Auxiliary Lane [Eastbound])

-Future Conditions-

2035 AADT: 78,100

2035 Truck AADT: 2.6% (2,030 trucks per day)

LOS F (8 Lanes)

Military Trail (South of PGA Boulevard)

-Existing Conditions-

2011 AADT: 37,000

2011 Truck AADT: 4.7% (1,739 trucks per day)

LOS C (6 Lanes)

-Future Conditions-

2035 AADT: 59,100

2035 Truck AADT: 4.7% (2,778 trucks per day)

LOS F (6 Lanes)

Sources:

- (1) 2011 AADT and 2011 Truck AADT volumes obtained from the FDOT's Florida Traffic Online (2011).
- (2) Projected 2035 AADT volumes derived from the Southeast Regional Planning Model (SERPM) Version 6.5.2e.
- (3) Projected 2035 Truck AADT volumes are based on the assumption that future truck traffic percentages are consistent with the 2011 existing percentages.
- (4) LOS derived from the FDOT 2009 Quality/Level of Service Handbook: Generalized Annual Average Daily Volumes for Florida's Urban Areas, Table 1.

It should additionally be noted that the Palm Beach MPO 2035 Long Range Transportation Plan (LRTP) states that volume to capacity (V/C) ratios exceeding 1.1 are assumed to constitute a travel demand need or deficiency. Based on the projected 2035 AADT volumes derived from the Southeast Regional Planning Model (SERPM), PGA Boulevard and the interchange at I-95 are expected to have a V/C ratio greater than 1.1 and are, therefore, projected to be deficient in the future if no improvements are made.

GROWTH MANAGEMENT: Accommodate Future Population and Employment Growth

The study area is urbanized containing a mixture of commercial, industrial, mixed-use and residential land uses with vacant land in the northeast quadrant. According to the City of Palm Beach Gardens Comprehensive Plan, future land use is to remain relatively unchanged, with the exception of the area east of the interchange which has been designated as part of the Bioscience Research Protection Overlay (BRPO). The BRPO was developed to protect portions of land for biotechnology/biosciences land uses and includes the Scripps Florida Phase II/Briger Tract DRI which consists of 82 acres located south of Donald Ross Road, north of Hood Road and east and west of I-95 (just north of the study area). The DRI includes



1,600,000 square feet of Biotech Research and Development, 2,400,000 square feet of biotechnological/biomedical, pharmaceutical, and office space, 2,700 residential dwelling units, and 500,000 square feet of retail space.

According to SERPM projections developed for Palm Beach County as part of the Palm Beach MPO 2035 LRTP development:

- Population is projected to grow from 1,270,302 in 2005 to 1,677,170 in 2035 [32% increase].
- Employment is projected to grow from 544,496 in 2005 to 800,045 in 2035 [46.9% increase].

The improvements will be critical in supporting the growing bioscience industry and vision of the County, as well as the expanding residential, commercial and industrial uses in the vicinity of the interchange.

SECONDARY CRITERIA

MODAL INTERRELATIONSHIPS: Enhance Freight Mobility

I-95 is the primary interstate route along the east coast of the United States extending from Maine to Florida and serving some of the most populated urban areas in the country. In Florida, I-95 is both a designated Strategic Intermodal System (SIS) highway and a major facility of Florida's Intrastate Highway System (FIHS). The SIS is a statewide network of highway, railway and waterway corridors as well as transportation hubs that handle the bulk of Florida's passenger and freight traffic. Highways that are designated as part of the SIS provide for movement of high volumes of goods and people at high speeds. The Florida Intrastate Highway System (FIHS) is composed of interconnected limited- and controlled-access roadways (which include designated SIS highway corridors) that provide for high-speed and high-volume traffic movements within the state to serve both interstate and regional commerce and long-distance trips. This statewide transportation network accommodates high occupancy vehicles, express bus transit and, in some corridors, passenger rail service. Within southeast Florida, I-95 is a vital north-south transportation corridor providing important regional access to major east/west and north/south transportation corridors, as well as residential and employment activity centers and other regional destinations in the area.

The proposed new interchange at I-95 and Central Boulevard and the mainline improvements between Military Trail and Central Boulevard are critical to enhance the mobility of goods by alleviating current and future congestion at the interchange and on the surrounding freight network. Reduced congestion will serve to maintain and improve viable access to the major transportation facilities and businesses of the area (including connectors to freight activity centers/local distribution facilities or between the regional freight corridors).

EMERGENCY EVACUATION: Enhance Emergency Evacuation and Response Times

I-95 and PGA Boulevard serve as part of the emergency evacuation route network designated by the Florida Division of Emergency Management. Also designated by Palm Beach County and the City of Palm Beach Gardens as evacuation facilities, I-95 and PGA Boulevard are currently critical in facilitating traffic during emergency evacuation periods as they connect other major arterials and highways of the state evacuation route network. Construction of a new interchange at Central Boulevard is anticipated to:

- Improve emergency evacuation capabilities by enhancing connectivity and accessibility to I-95 and other major arterials designated on the state evacuation route network.
- Increase the operational capacity of traffic that can be evacuated during an emergency event.
- Reduce demand at the existing I-95/PGA Boulevard interchange.

2.4 EXISTING FACILITY

Within the study area, SR 9/I-95 is a ten-lane divided, limited access facility. The speed limit is 70 mph north of PGA Boulevard. Central Boulevard is a four-lane divided collector road. The speed limit is 45 mph. The existing typical sections for I-95 and Central Boulevard are described below.

SR 9/I-95 South of Central Boulevard (from the PGA Boulevard ramps to Central Boulevard overpass)

Figure 3 depicts the existing roadway typical section for I-95 south of Central Boulevard. This section provides four 12-foot wide general purpose lanes, one 12-foot wide auxiliary lane, and a 15-foot inside and 12-foot outside shoulder in each direction. The northbound and southbound lanes are separated by 32-foot median which contains a concrete barrier. The 12-foot auxiliary lanes are not continuous throughout the section. The roadside swales vary from 60 feet to 150 feet. The maximum width of the typical section is 300 feet.

SR 9/I-95 north of Central Boulevard (from Central Boulevard to Donald Ross Road)

Figure 4 depicts the existing roadway typical section for I-95 north of Central Boulevard. This typical consists of four 12-foot wide general purpose lanes, two 12-foot wide auxiliary lanes, and a 14-foot inside and 12-foot outside shoulder in each direction. The northbound and southbound lanes are separated by a 28-foot grassed median (excluding the shoulders) and a double faced guardrail. The auxiliary lanes are not continuous throughout the section. The roadside swales vary from 60 feet to 146 feet. The maximum width of the typical section is 372 feet.

Central Boulevard

Figure 5 depicts the existing roadway typical section for Central Boulevard approaching the bridge over I-95. Two 12-foot through lanes with a 10-foot wide outside shoulder are provided in each direction.

The eastbound and westbound lanes are separated by a 22-foot raised median. An eight-foot wide sidewalk is provided on the west side and a five-foot wide sidewalk is provided on the east side of Central Avenue. The area between the outside of the sidewalk and the outer edge of the right-of-way varies from three to 98 feet. The total width of the typical section for this segment of Central Boulevard varies from 120 to 265 feet.

Figure 3– Existing I-95 Roadway Typical Section – South of Central Boulevard

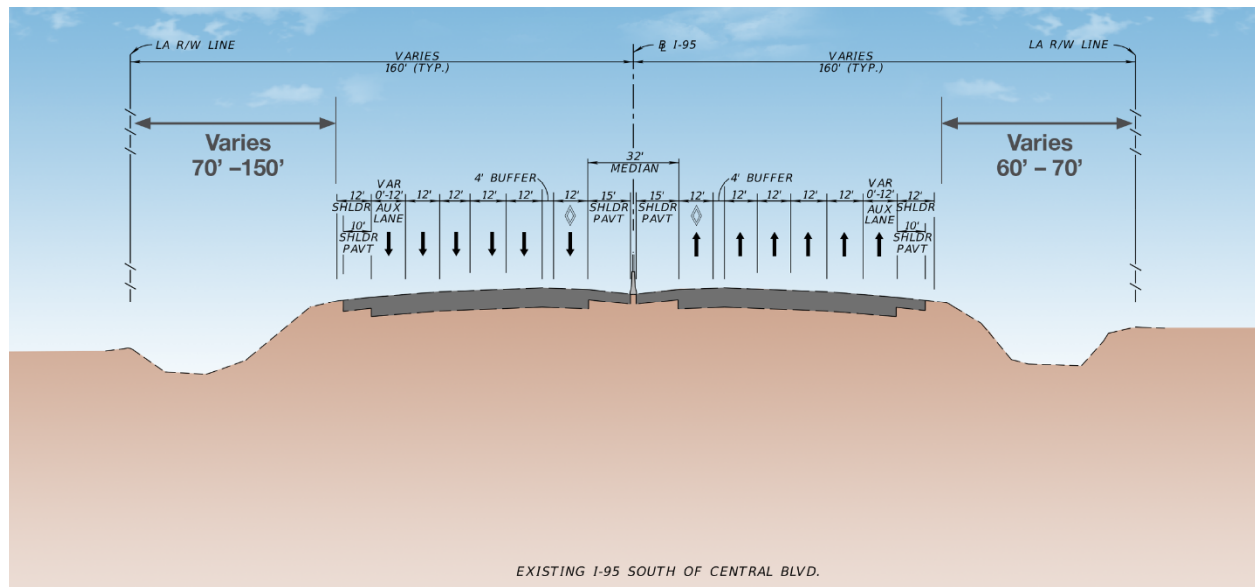
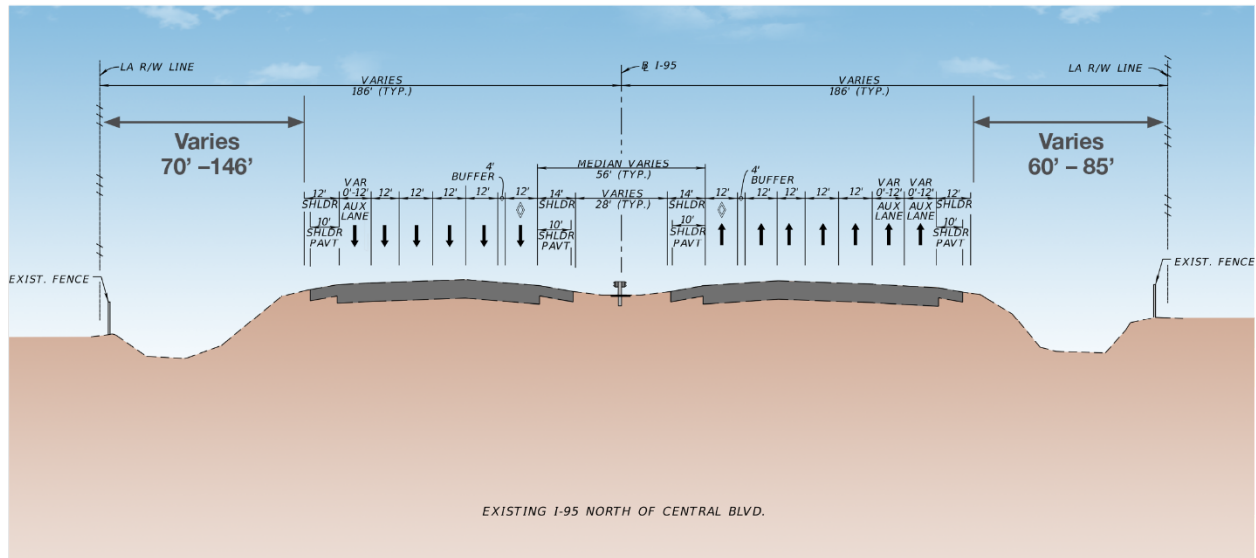


Figure 4– Existing I-95 Roadway Typical Section – North of Central Boulevard



Central Boulevard Bridge over SR 9/I-95

Figure 6 depicts the existing bridge typical section for the Central Boulevard Bridge over I-95. Two 12-foot through lanes with a 10-foot wide outside shoulder are provided in each direction. An eight-foot wide sidewalk is provided on the west side and a five-foot wide sidewalk is provided on the east side of Central Avenue. The eastbound and westbound lanes are separated by a 22-foot median (19 feet raised). The total out-to-out width of the existing bridge is 107 feet-six inches.

Figure 5– Existing Central Boulevard Roadway Typical Section

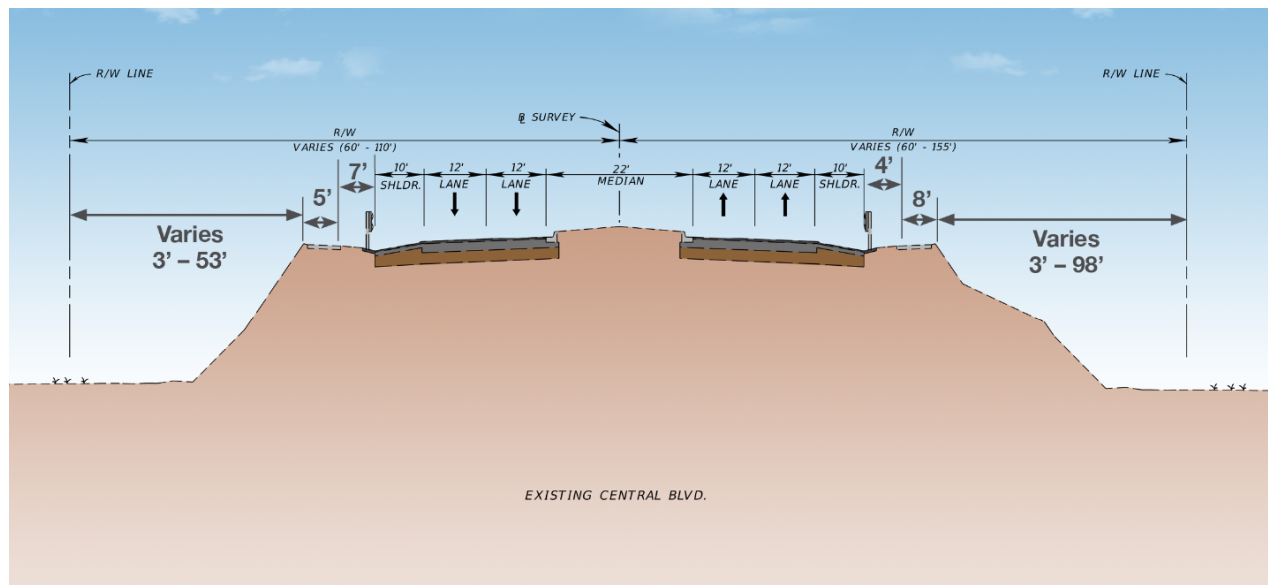
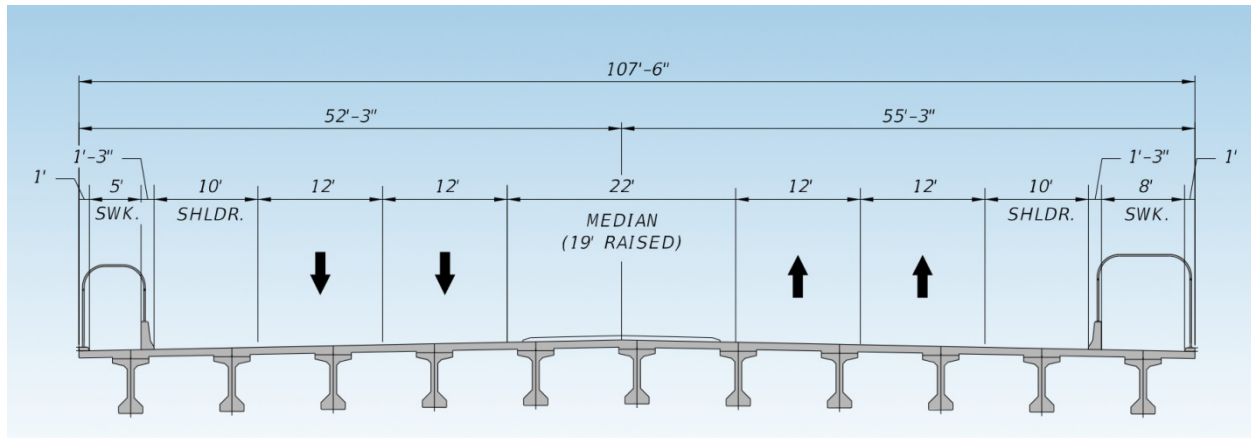


Figure 6– Existing Central Boulevard Bridge Typical Section



2.5 ALTERNATIVES CONSIDERED

Alternatives evaluated during the PD&E Study include the No-Build Alternative and two build alternatives. The No-Build Alternative will remain viable until after the Public Hearing. Over 20 build alternatives were evaluated as part of the IJR preceding this PD&E Study.

The advantages of the No-Build Alternative include the following:

- No disruption to motorists during construction,
- No additional noise impacts,
- No wetland or wildlife impacts,
- No temporary construction impacts, or disruption to motorists during construction,
- No additional right-of-way impacts, and
- No impacts to the Palm Beach County planned District Park.

The disadvantages of the No-Build Alternative include the following:

- Congestion within the project limits will not be reduced,
- Operational capacity will not be improved during emergency evacuations,
- Traffic Demand will continue to increase at the existing I-95/PGA Boulevard Interchange, and
- Mobility will not be improved within the City of Palm Beach Gardens.

Two interchange options for each build alternative are under consideration. Alternatives 2 and 3 include construction of a new tight diamond urban interchange (TDUI) at Central Boulevard and I-95. Alternatives 2A and 3A include construction of a new Diverging Diamond Interchange (DDI). Descriptions of these build alternatives are provided below. Both require varying amounts of Right of Way acquisition. The alternative concept plans are included in Appendix A.

The TDUI interchange consists of one-way diagonal ramps in each quadrant of the interchange that are designed to minimize impacts to the existing right-of-way. The ramp terminals from the I-95 mainline to Central Boulevard will be signalized and consist of one left turn lane and two right turn lanes in each quadrant. The on-ramps from Central Boulevard to the I-95 mainline will consist of two signalized left turn lanes and a free-flow right turn one-lane ramp.

The DDI alternative requires drivers to briefly cross to the left, or opposite side of the road at carefully designed crossover intersections. Drivers will travel for a short distance, then cross back to the right side of the road. The design allows for free-flow movements for the left and right turns to and from the I-95 ramps onto Central Boulevard without crossing the path of opposing traffic. This interchange does not require a signal for left turning vehicles, thus allowing more green time for opposing traffic. This design will, however, require the construction of two new bridges in order to accommodate the necessary geometry and acquisition of additional right-of-way.

2.5.1 BUILD ALTERNATIVES 2, 2A

Alternative 2 includes a new TDUI at Central Boulevard and a collector-distributor (CD) roadway system adjacent to northbound and southbound SR 9/I-95 between the Military Trail ramps and the Central Boulevard interchange ramps. This alternative removes the direct connection of the ramps at Military Trail to I-95. Northbound I-95 on ramp traffic at Military Trail merges with northbound I-95 off ramp traffic at Central Boulevard, and the weaving movement between the two occurs on the northbound collector road. Similarly, southbound I-95 on ramp traffic from Central Boulevard merges with southbound I-95 off ramp traffic at Military Trail, and the weaving movement between the two occurs on the on the southbound collector road. Alternative 2A is essentially the same as Alternative 2, except that a DDI is proposed.

2.5.2 BUILD ALTERNATIVES 3, 3A

Alternative 3 includes a new TDUI Central Boulevard. This alternative also includes braided ramps between Military Trail and Central Boulevard to eliminate the weaving sections in this area. The I-95 northbound off ramp to Central Boulevard passes over top of the I-95 northbound on ramp from Military Trail. The I-95 southbound off ramp to Military Trail passes over top of the I-95 southbound on ramp from Central Boulevard. This alternative differs from Alternative 2 only in the treatment of ramp maneuvers on I-95. Alternative 3A is essentially the same as Alternative 3, except that a DDI is proposed.

2.6 RECOMMENDED ALTERNATIVE

Evaluation Matrices were developed to facilitate comparison of traffic operation and engineering issues; construction costs and right-of-way impacts; socio-economic, natural and physical environmental impacts; and public input for the four viable alternatives. Based on comparative analysis of the four alternatives, the project team selected Alternative 2 as

the Recommended Alternative. Alternative 2 combines the CD roadway system adjacent to northbound and southbound SR 9/I-95 between the Military Trail ramps and the Central Boulevard interchange ramps with construction of a new TDUI at Central Boulevard.

The proposed typical section for I-95 south of Central Boulevard for the CD road alternative is shown in **Figure 7**. This section includes four 12-foot wide general purpose lanes and one 12-foot wide special use lane, a 15-foot inside shoulder, and a 12-foot outside shoulder in each direction. A continuous 12-foot wide auxiliary lane in each direction is also provided. The north and southbound lanes are separated by a two-foot wide concrete median barrier.

The proposed CD road is separated from the mainline by a grassed median that varies in width from six feet to 55 feet. Three 12-foot wide through lanes, with 12-foot wide inside and outside shoulders are provided. The swales at the edges of the right-of-way vary in width from 22 feet to 42 feet. The total width of the typical section, including the CD road, is 441 feet.

The proposed typical section for I-95 north of Central Boulevard is shown in **Figure 8**. This typical section is the same for Mainline Alternative 3. The typical section consists of four 12-foot wide general purpose lanes, one 12-foot wide special use lane, and a 14-foot inside and a 12-foot outside shoulder in each direction. Two southbound 12-foot auxiliary lanes are provided in each direction. Northbound and southbound lanes are separated by a 28-foot grassed median and a double faced guardrail. The swales at the edges of the right-of-way vary in width from 69 feet to 145 feet. The maximum total right-of-way required for this proposed typical section is 372 feet.

Figure 7– Typical Section – I-95 South of Central Boulevard (Mainline Alternative 2)

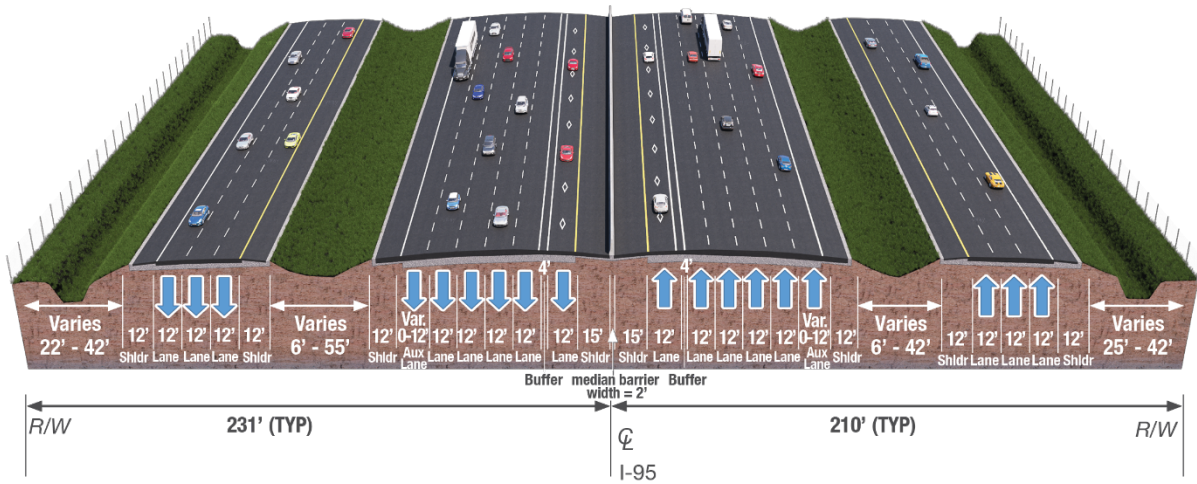
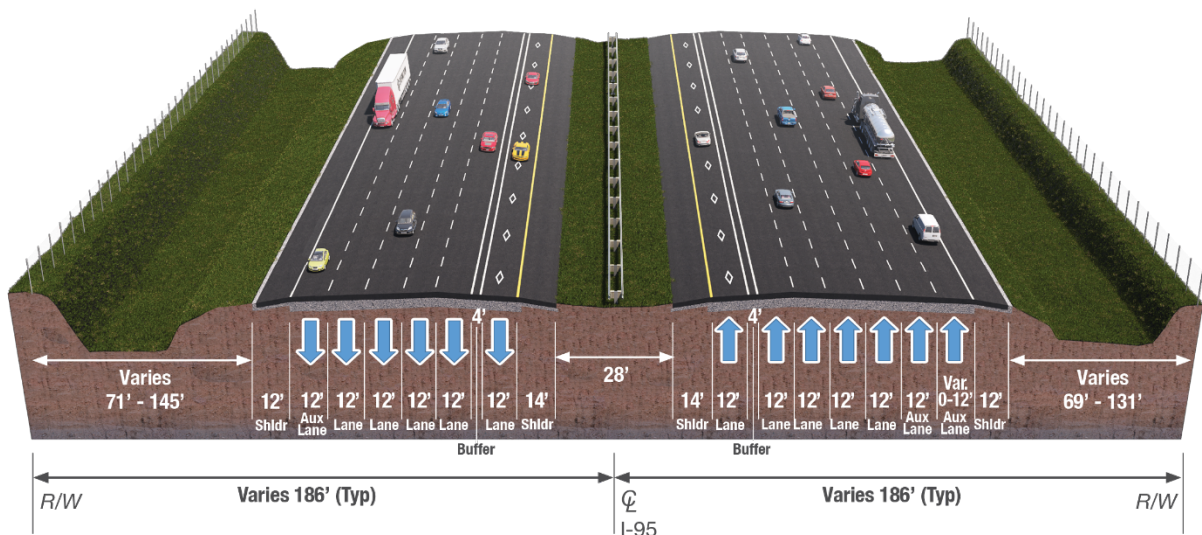


Figure 8– Typical Section – I-95 North of Central Boulevard (Mainline Alternatives 2 and 3)



The typical section for the proposed Central Boulevard Bridge for the proposed TDUI at Central Boulevard is shown in Figure 9. This section provides two 11-foot wide through lanes, two 11-foot left turn lanes, a seven-foot designated bicycle lane, and a ten-foot wide enclosed sidewalk in each direction, separated by a four-foot traffic separator. The out-to-out width of the proposed bridge is 130 feet six inches.

The proposed typical section for Central Boulevard east of I-95 is shown in Figure 10. In the eastbound direction, this section provides two 11-foot through lanes, a seven-foot designated bicycle lane and an eight-foot sidewalk. In the westbound direction this section provides four 11-foot through lanes, one 11-foot auxiliary lane, a seven-foot wide designated bicycle lane, and a ten-foot wide sidewalk separated from the travel lanes by a pedestrian rail. The eastbound and westbound lanes are separated by a grassed median that varies in width from 13 feet to 27.5 feet. The total width of this typical section varies from 120 feet to 253 feet.

The proposed typical section for Central Boulevard west of I-95 is shown in Figure 11. In the eastbound direction, this section provides two 11-foot through lanes, a seven-foot designated bicycle lane and an eight-foot sidewalk. In the westbound direction, this section provides four 11-foot through lanes, one 11-foot auxiliary lane, a seven-foot wide designated bicycle lane, and a ten-foot wide sidewalk separated from the travel lanes by a pedestrian rail. The east and westbound lanes are separated by a grassed median that varies in width from 13 feet to 27.5 feet. The total width of this typical section varies from 120 feet to 265 feet

Figure 9— Typical Section - Central Blvd. Bridge for TDUI

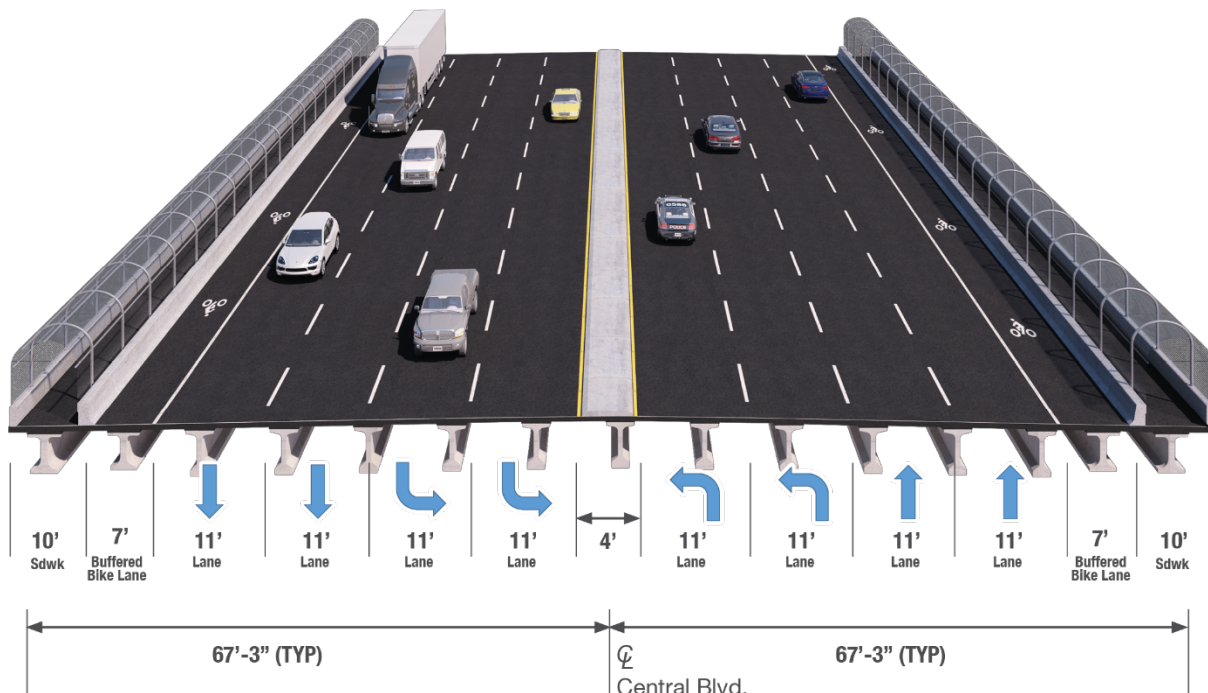


Figure 10- Proposed Typical Section - Central Blvd. East of I-95 – TDUI

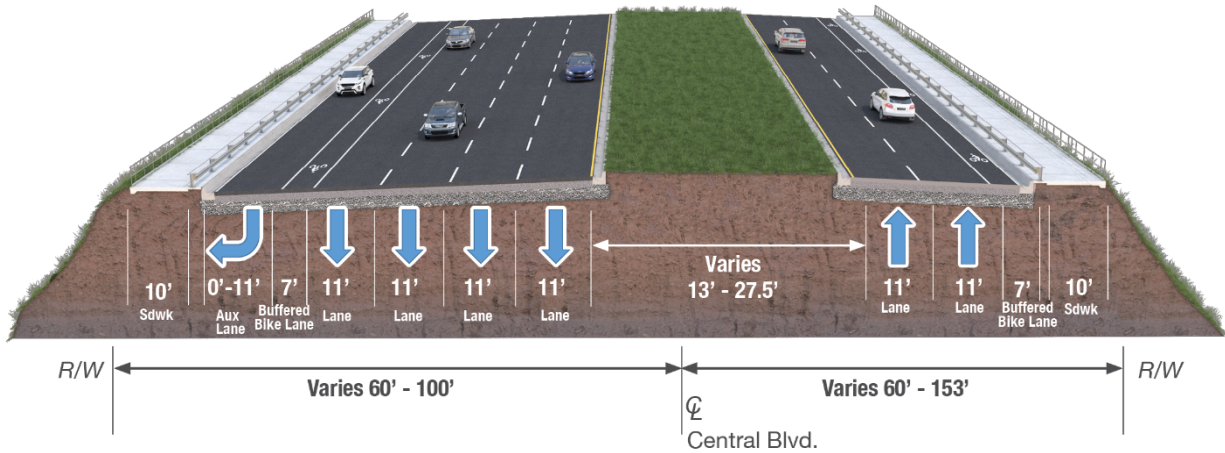
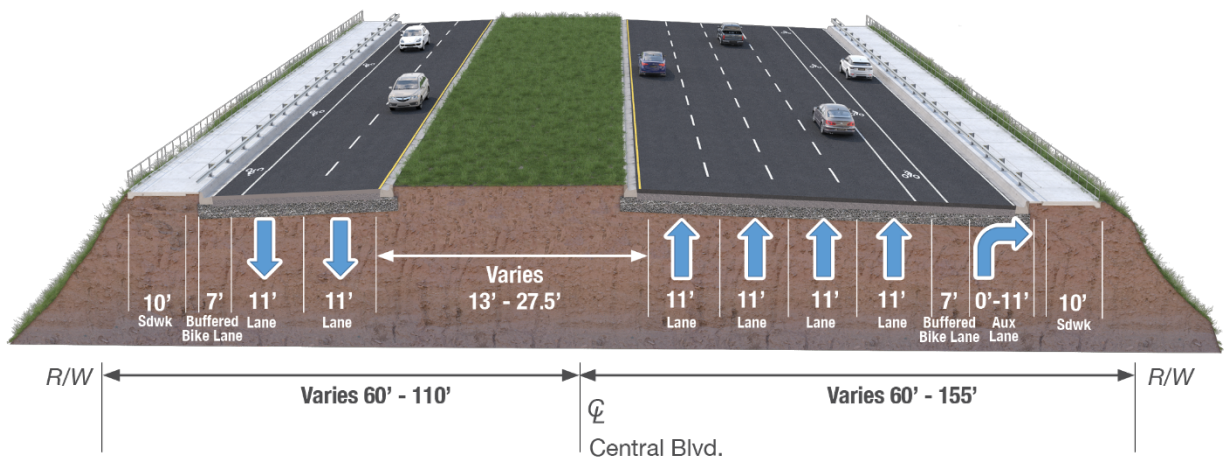


Figure 11– Proposed Typical Section - Central Blvd. West of I-95 – TDUI





It is anticipated that acquisition of approximately 11.34 acres of right-of-way would be required for construction of the Recommended Alternative 2. No business or residential relocations will be required. Environmental impacts are anticipated to be minimal. The estimated total construction cost for Alternative 2 is approximately \$33.9 million.

The Recommended Alternative will meet the purpose and need of the project, have minimal environmental impacts, requires acquisition of the least amount of additional right-of-way, and is the most acceptable to the community. Construction costs for Alternative 2 are estimated to be the lowest of the four build alternatives evaluated.

3.0 COMMUNITY CHARACTERISTICS INVENTORY AND IMPACT ANALYSIS

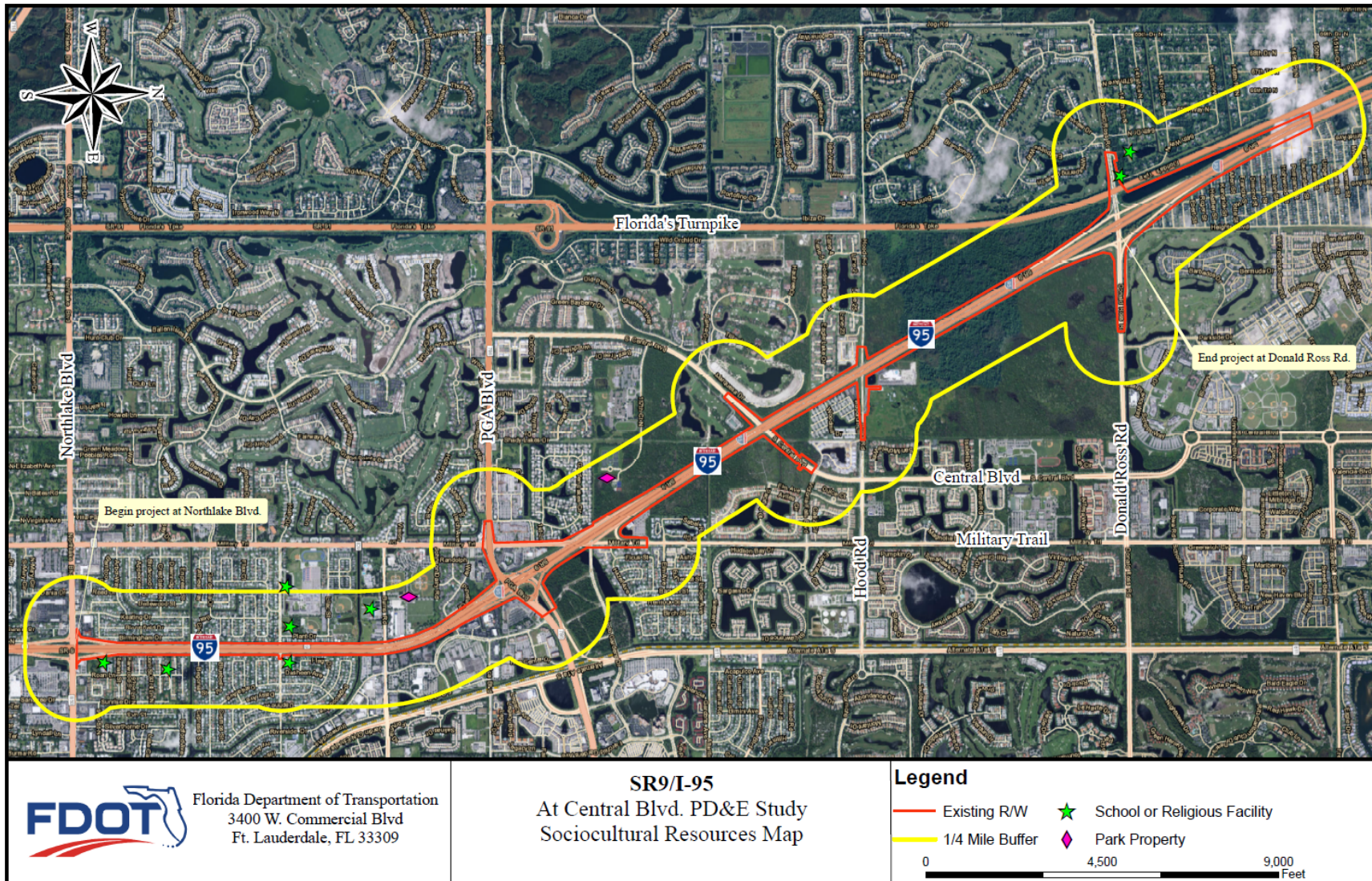
The Socio-Cultural Effects Evaluation (SCE) process is supported by the development of a Community Characteristics Inventory (CCI) for each defined community within the study area. The CCI is a comprehensive summary of the quantitative and qualitative data used to support the decisions made during the SCE Evaluation process. The CCI is used to acquire a better understanding of the affected community and potential issues considered in an effort to evaluate the effect of a transportation action of the community. A CCI is valuable to the identification and later resolution of issues.

3.1 STUDY AREA

The study area is defined as the geographic areas that include all communities with the potential to be affected by a transportation action. The study area typically includes communities immediately surrounding the project but may also extend beyond the typical project corridor.

The SCE report evaluated potential/existing community resources within the larger study area included in the Interchange Justification Study along SR 9/I-95 from north of Northlake Boulevard to south of Donald Ross Road (see Error! Reference source not found.). The current project limits are located entirely within this study area, which contains all sociocultural resources within and adjacent to the recommended build alternative. **Figure 12**, shows the locations of sociocultural resources within 1,320-ft from the I-95 centerline, from Northlake Boulevard at the southern limit to Donald Ross Road at the northern limit, and from Florida's Turnpike to the west to Lake Victoria Gardens Boulevard to the east. A 1,320-ft buffer is used as it is the largest recommended buffer width from the Environmental Screening Tool (EST). No relocations are anticipated and potential adverse socio-cultural effects are not likely to occur.

Figure 12– Socio-Cultural Effects Study Area



3.1.1 SOCIAL

How a transportation project impacts social issues is a function of the existing communities in the project study area. These potential social issues may impact community cohesion, demographics, safety and emergency response times, compatibility with community goals and quality of life.

Communities

A community is defined by geographic, manmade or natural, boundaries with respect to both people and places. The people who comprise a community may share similar social, cultural, ethnic, economic, political, or religious characteristics, as well as common histories. They may attend the same schools, churches, or social clubs. These people may intersect in social settings and share similar values. The project corridor is located within the City of Palm Beach Gardens, in Palm Beach County (as illustrated in the Project Location Map in **Figure 1** and **Figure 2**).

The following tables identify the community facilities that are located within the SCE study area.

Schools

Table 3-1 Schools	
Name	Address
Barry University - North Palm Beach Campus	9123 N Military Trail
Howell L Watkins Middle School	9480 Macarthur Blvd
Trinity Christian School Of Palm Beach Gardens	9625 N Military Trail
Palm Beach Gardens High School And Adult Education	4245 Holly Drive
Palm Beach Gardens Elementary School	10060 Riverside Drive
Nativity Lutheran Church & School	4075 Holly Drive
Church In The Gardens School	3937 Holly Drive
The Weiss School	4176 Burns Road
Riverside Partners, LLC, Lessor	10300 Riverside Drive

Table 3-1 Schools	
Name	Address
Bright Futures International - Riverside Campus	10350 Riverside Drive
Saint Mark's Episcopal School	3395 Burns Road
Palm Beach Community College	3205 RCA Blvd; 11051 Campus Drive
Watson B Duncan Middle School	5150 117th Ct North
Benjamin Private School, Inc.	4875 Grandiflora Road
William T Dwyer High School And Adult Education	13601 N Military Trail
Marsh Pointe Elementary School	12649 Ibiza Drive
School Two Inc., Lessor	11500 N Jog Road
University Of Phoenix- West Palm	7111 Fairway Drive
Palm Beach State College - Palm Beach Gardens	3160 PGA Boulevard
Beacon Cove Intermediate School	150 School House Road
Florida Atlantic University - JD MacArthur Campus	5353 Parkside Drive
Sunshine Tree School	6327 Donald Ross Road
Timber Trace Elementary School	5200 117th Ct North
Independence Middle School	4001 Greenway Drive
Holland Northlake Day School	4650 Northlake Boulevard

Table 3-2 Daycare Facilities	
Name	Address
The Children's Academy At Cove	9153 Roan Land
Nativity Lutheran Church & School	4075 Holly Drive
Church In The Gardens, Inc.	3937 Holly Drive
Riverside Youth Enrichment Cen	10170 Riverside Drive
The Weiss School	4176 Burns Road
Chesterbrook Academy	3932 RCA Boulevard
Temple Beth David	4657 Hood Road
Sunshine Tree Child Care Center	6327 Donald Ross Road
Kamae Haltaufderhyde (owner or facility manager)	5369 Eagle Lakes Drive
Mary Stelwagon (owner or facility manager)	9160 Demery Drive

Emergency and Medical Facilities

Healthcare facilities, nursing homes, law enforcement, fire departments, etc.

Table 3-3 Healthcare Facilities	
Name	Address
Gardens Urgent Care	3555 Northlake Boulevard
Grace Medical Center Of Florida Inc.	4212 Northlake Boulevard
Northlake Medical Center	4300 Northlake Boulevard
Gardens Health & Wellness	4383 Northlake Boulevard
MD Now Medical Centers Inc.	9060 N Military Trail
Palm Beach Medical Clinic	9123 N Military Trail
Jstadoc, Inc. / MCCI Group Holdings	9121 N Military Trail
North County Surgicenter	4000 Burns Road
Ahner Health & Medical Center	10333 N Military Trail
Powers Chiropractic Center	10625 N Military Trail
Palm Beach Dermatology / Berto Lopez, M.D., P.A. / Youthful Balance Medical Center / Vanaja Sureddi, M.D., P.A.	10887 N Military Trail
Emergency Care Service Of JFK Medical	4797 PGA Boulevard
Minute Clinic	11000 N Military Trail
Garden Dermatology	11030 RCA Center Drive
Gardens Plastic Surgery / Evan R. Shapiro, M.D.	11020 RCA Center Drive
Palm Beach Institute For Cosmetic Surgery & Longevity	4060 PGA Boulevard

Table 3-3 Healthcare Facilities	
Name	Address
Laser Skin Solutions / Palm Beach Facial Plastic Surgery, LLC / Kotzen Center for Women's Health	4280 Professional Center Drive
Neurosurgery Clinic of the Palm Beaches	4290 Professional Center Drive
Dermatology Associates P.A. of the Palm Beaches	10335 N. Military Trail

Table 3-4 Nursing Homes	
Name	Address
Alzheimer Community Care	4075 Holly Drive
Highlands Care	9222 E. Highland Pines Drive
Assisted Living of Palm Beach	9239 W. Highland Pines Drive

Table 3-5 Law Enforcement	
Name	Address
Palm Beach Gardens Police Department	10500 N. Military Trail

Table 3-6 Fire Departments	
Name	Address
Palm Beach Gardens Fire Department and Rescue Station 1	4425 Burns Road

Other Community Facilities

Community Centers, Religious Facilities, Other Social Services, and Government Buildings

Table 3-7 Community Centers	
Name	Address
Lions Club – Palm Beach County North	10800 N. Military Trail
Masonic Lodge – Garden 366 F & AM	9463 Roan Lane
Burns Road Community Center	4404 Burns Road
Palm Beach Gardens City - Community Services - Lakeside Center	10410 North Military Trail

Table 3-8 Religious Facilities	
Name	Address
Chabad of Palm Beach Gardens	11701 Lake Victoria Gardens Avenue
Nativity Lutheran Church	4075 Holly Drive
Church in the Gardens	3937 Holly Drive
Trinity United Methodist Church	9625 N. Military Trail
St. Ignatius Loyola Church – Catholic Diocese of Palm Beach	9999 N. Military Trail
Covenant Centre International	9153 Roan Lane
Palm Beach Counseling Center	3970 RCA Boulevard
Palm Beach Community Church	4885 PGA Boulevard

Table 3-9 Other Social Services	
Name	Address
Young Body Rehabilitation	9091 N. Military Trail
McEwen, David S., DC PA - Res	4074 Lakespur Circle North
Marino, Dominick A. OD PA	4530 PGA Boulevard, Suite 105
Divorce Mediation Specialists	4400 PGA Boulevard
Thurston, Nancy, LCSW	4360 Northlake Boulevard
ERE Associates	3450 Northlake Boulevard
Gentlemen Of The Garden, Inc.	4400 PGA Boulevard
Chip Child ID Program	4653 Spruce Lane
Gottlieb, Evelyn, LCSW, Psychotherapist	9121 North Military Trail
School District Of Palm Beach County -Schools, Roosevelt Elementary, Parent Connection	4245 Holly Drive
Christian, Raymond M., PA	4114 Northlake Boulevard
Swiss House	3927 Buttercup Circle South
Palm Beach Counseling Center	3970 RCA Boulevard
Community Christian Counseling Center	9625 North Military Trail
Palm Beach Gardens Baseball Hotline	4405 Burns Road
Helping Hands Scholarship Fund, Inc.	9995 N Military Trail
Goodwill Industries	4224 Northlake Boulevard

Table 3-10 Government Buildings	
Name	Address
City of Palm Beach Gardens Municipal Complex	10500 N. Military Trail

Cultural Resources

Cultural resources or “historic properties,” as defined by the National Historic Preservation Act (NHPA) of 1966 (Public Law 89-655, as amended), are any “prehistoric or historic district, site, building, structure, or object included in, or eligible for inclusion in the National Register of Historic Places (NRHP).” The term “historic structures” includes bridges, houses, buildings, and constructed features which, with few exceptions, are at least fifty years old. Historic districts consist of historic buildings and other elements which retain identity and integrity as a whole. Cemeteries and burial places are also considered cultural resources. Archaeological sites, also referred to as “archaeological resources,” represent the locations of prehistoric or historic activities. Cultural resources serve as tangible expressions of shared community values and help define the character of a community or neighborhood.

Potential involvement with cultural resources was studied separately in the Cultural Resource Assessment Survey (CRAS) prepared for this project on file at FDOT District 4. The purpose of the CRAS was to locate and identify any archaeological sites and historic resources within the project Area of Potential Effects (APE) and to assess their significance in terms of eligibility for listing in the NRHP.

The CRAS was conducted in compliance Section 106 of the NHPA of 1966, as implemented by 36 CFR 800 (Protection of Historic Properties, effective January 2001); Chapter 267, Florida Statutes; Section 4(f) of the Department of Transportation Act of 1966, as amended (49 USC 303); and the minimum field methods, data analysis, and reporting standards embodied in the Florida Division of Historical Resources’ (FDHR) Historic Preservation Compliance Review Program (November 1990), Cultural Resource Management Standards and Operational Manual (February 2003), and Chapter 1A-46 (Archaeological and Historical Report Standards and Guidelines), Florida Administrative Code. In addition, the CRAS was prepared in conformity with standards set forth in Part 2, Chapter 12 (Archaeological and Historic Resources) of the FDOT Project Development and Environment Manual (revised, January 1999). All work conformed to professional guidelines set forth in the Secretary of Interior’s Standards and Guidelines for Archaeology and Historic Preservation (48 FR 44716, as amended and annotated).

No archaeological sites were identified during the survey. Background research indicated that the archaeological APE has been heavily altered by urban development and has a low potential for containing archaeological sites. One shovel test was excavated within the archaeological APE. No archaeological material was identified. The pedestrian survey and subsurface testing confirmed the low archaeological site potential of the archaeological APE.

The historic resources survey resulted in the identification of one previously recorded 19th century Seminole footpath and military trail (8PB13795), two newly identified historic buildings (8PB16283 and 8PB16284), and two newly identified historic canals (8PB16285 and 8PB16286). No evidence of the 19th century military trail was identified during the survey and the portion of the resource within the APE is considered ineligible for listing in the National Register due to its lack of integrity.

The newly recorded Dog Days building located at 4052 Burns Road (8PB16283) and Anspach Building/4500 Riverside Drive (8PB16284) are common vernacular style buildings that do not possess historical or architectural significance. Therefore, these buildings are considered ineligible for listing in the National Register individually or as part of the historic district. The newly recorded Earman River Relief Canal (8PB16285) and Earman River Canal Branch (8PB16286) are examples of common canals which do not exhibit significant engineering techniques. These canals are also considered ineligible for listing in the National Register individually or as part of a historic district.

Public Parks and Recreational Areas

Section 4(f) of the U.S. Department of Transportation Act of 1966 protects public parks, recreation areas, wildlife and waterfowl refuges, and historic sites. There are eight publicly owned parks and recreational areas within the SCE study area that are potentially subject to Section 4(f) protection. Table 3-11 identifies the parks and recreational areas located within the SCE study area.

Table 3-11 Parks and Recreational Areas	
Name	Address
Palm Beach Gardens City Park/Tennis Center	5070 117 th Court North
Palm Beach County District Park (future)	Shady Lakes Boulevard & 117th Court North
South Ilex Circle	4158 Ilex Circle South
Burns Road Community Park	4404 Burns Road
Thompson River Linear Park	Military Trail
Gardens Park	4301 Burns Road
Plant Drive Park	Plant Drive
Lilac Park	4175 Lilac Street

Physical Barriers

Physical barriers are natural or man-made obstructions to the interconnectivity between or within communities. I-95 functions as a man-made obstruction to the interconnectivity between communities on either side of the roadway. No natural obstructions to the interconnectivity between or within communities occur within the SCE study area.

3.1.1.1 DEMOGRAPHICS

Demographic data describes the community's population. It is primarily collected by local, state or federal agencies such as the Census Bureau and other local government departments. The data includes a range of topics about people in communities, such as population size, gender, age composition, ethnic backgrounds, household characteristics, and geographic distribution. Such data assists in the design of public participation, outreach, and education strategies that reflect the various age, educational and economic backgrounds present in the community.

Increases and decreases in population are not expected as a result of this project. The population is expected to increase or decrease in response to regional factors unrelated to the project and it is anticipated that any future growth in the study area will be in accordance with the Palm Beach County Comprehensive Plan. Additionally, as residential or

business relocations are not anticipated, displacement of minority populations is not anticipated as a result of the Recommended Alternative.

Table 3-12 2010 IJR Study Area Demographics		
Race	Population	Percentage
White*	5,355	75.76
Black*	1,029	14.56
Native American	0	0.00
Asian	299	4.23
Other	111	1.57
TOTAL	6,794	

*Hispanic (includes Black and White): 1,017; 14.39%

Demographic information concerning education and age may indicate that electronic/internet communication, such as a project website or on-line survey, may not be appropriate for a specific community. The number of individuals who speak less than proficient English can also be determined, and public information can be provided in an appropriate language. Executive Order 13166 "Improving Access to Services for Persons with Limited English Proficiency (LEP)" was signed into law by President Clinton in 2011, to ensure that people with LEP have meaningful access to programs and activities of agencies receiving federal financial assistance.

The United States Department of Transportation (USDOT) published Policy Guidance Concerning Recipients' Responsibilities to LEP Persons in December 2005. The Safe Harbor guidance within the USDOT Guidance Document (Section VI: Selecting Language Assistance Services. B: Written Language Services (Translation), Safe Harbor), with which FDOT complies, states that adherence to the following will provide strong evidence that the recipient's action is in compliance with Title VI written-translation obligations: (a) "...provides written translations of vital documents for each eligible LEP language group that constitutes 5% or 1,000, whichever is less, of the population of persons eligible to be served or likely to be affected or encountered." It should be noted that 345 persons within the 1,320-foot buffer (3.8% of the total buffer population) indicated a deficiency in English proficiency. LEP accommodations are not necessary at this time pursuant to Part 1, Chapter 11, Section 11-1.2.4 of the FDOT PD&E Manual. **Table 3-12** summarizes demographic information about the population within the SCE study area. This information was obtained

from the Efficient Transportation Decision Making (ETDM) tool, which is derived from the 2010 census data.

3.1.1.2 COMMUNITY COHESION

As part of community cohesion, residents have a sense of belonging to their neighborhood or community, including commitment to the community or level of attachment to neighbors, institutions in the community, or particular subgroups. Community cohesion also includes the degree of social networking in a community, including the degree to which residents cooperate and interact. The project is located along the existing I-95 corridor within the City of Palm Beach Gardens in Palm Beach County. The limits of the project are shown in **Figure 2**.

I-95 is an existing limited access facility. The proposed improvement will reduce congestion and improve local and regional mobility. The proposed mainline improvements and new interchange will not change the relationships of the existing communities on either side of the facility. The project is not anticipated to have effects on community cohesion, create isolated areas, disrupt social relationships and patterns or affect connectivity to community activity centers.

3.1.1.3 SAFETY AND EMERGENCY RESPONSE

The SCE Evaluation includes the effects of the transportation project on neighborhood safety. In this context, the evaluation of safety considers whether or not residents feel safe in their neighborhood and includes issues ranging from emergency response times, access to community facilities, and livable community features. There are two emergency facilities, Palm Beach Gardens Police Department and Palm Beach Gardens Fire Department and Rescue Station 1, located to the west of the project corridor on N. Military Trail and Burns Road, respectively. While these police and fire rescue stations are not located within the SCE study area, they are located within a 2,640-ft buffer (1/2 mile).

I-95, PGA Boulevard, and Central Boulevard serve as part of the emergency evacuation route network designated by the Florida Division of Emergency Management. Also designated by Palm Beach County and the City of Palm Beach Gardens as evacuation facilities, I-95, PGA Boulevard, and Central Boulevard are critical in facilitating traffic during emergency evacuation periods as they connect other major arterials and highways of the state evacuation route network.

The proposed interchange will reduce local congestion at PGA Boulevard and as a result, this project is anticipated to improve emergency service response times in the area of the I-95 and PGA Boulevard Interchange, and the area of the proposed new interchange at I-95 and Central Boulevard. The project is also anticipated to improve emergency evacuation capabilities by enhancing connectivity and accessibility to I-95 and other major arterials designated on the state evacuation route network, and increase the operational capacity of traffic that can be evacuated during an emergency event.

3.1.1.4 COMMUNITY GOALS/QUALITY OF LIFE

All local governments in Florida are required to adopt a Comprehensive Plan. The Comprehensive Plan includes goals related to future land use, transportation, housing, recreation, and capital improvements. As transportation actions can affect communities and influence the quality of life of its citizens, it is important that potential impacts and benefits to community facilities, cultural resources, public parks and recreation areas, community cohesion, safety/emergency response, and compatibility with community goals and issues as described below be evaluated.

The Goals, Objectives, and Policies presented in the Palm Beach County Comprehensive Plan Elements reflect the directives of the citizenry and the Board of County Commissioners. These directives are:

- A. Redirect growth to the East where services and facilities can be provided and encourage the revitalization/redevelopment of the coastal communities.
- B. Through the implementation of a concurrency management system provide for orderly growth and provision of facilities and services to maintain the existing quality of life in an economical manner.
- C. Implement County-wide growth management strategies while providing the opportunities for flexibility within the Plan that recognize and maintain the diversity of lifestyles.

According to the Comprehensive Plan, it is the goal of Palm Beach County to provide an interconnected multimodal transportation system which moves people, goods, and services in a safe, efficient, convenient, and economical manner with minimal adverse impact to the environment, as well as protect human life and native ecosystems by controlling air pollution through initiatives for the protection of air quality.

3.1.2 ECONOMIC ISSUES

Economics is a key variable in trying to establish the profile of a community. The SCE handbook states that economic conditions and employment describes a community's economic history, current economic wellbeing, and potential resources. This information takes into account employment levels, types of jobs, per capita income, poverty, unemployment rates, the range of incomes in the community, and trends in employment opportunities (e.g., family-owned businesses versus national chain businesses). This project has been reviewed by the appropriate agencies in the ETDM Tool and has been assigned a summary degree effect of Enhanced.

3.1.2.1 COMMERCE

There are a variety of commercial and retail establishments that are located in the SCE study area immediately adjacent to the I-95 corridor and along the intersecting major roadways. Such establishments include, hotels including Inn of America and Doubletree of Palm Beach Gardens; grocery stores and other retail centers such as Publix and CVS pharmacy. These businesses, which are also important community resources, contribute to the quality of life, and provide employment opportunities for residents in the study area. Most employees and patrons accessing businesses within the SCE study area likely use I-95, or cross I-95 at one of its interchanges. **Table 3-13** identifies the top employment centers within Palm Beach County, and **Table 3-14** identifies the top employment centers within Palm Beach Gardens, according to data obtained from the Florida Department of Transportation and Palm Beach County Business Development Board.

As part of the Strategic Intermodal System (SIS) facility, I-95 is critical to the economic vitality of the state as I-95 promotes the movement of goods and passengers within the southeastern coast of Florida. The proposed improvements to mainline I-95 and construction of a new interchange at Central Boulevard will enhance the mobility of goods by alleviating current and future congestion at the interchange and on the surrounding freight network. Reduced congestion will serve to maintain and improve access to the major transportation facilities and businesses in the area (including connectors to freight activity centers/local distribution facilities or between the regional freight corridors).

The proposed Central Boulevard I-95 interchange could have an impact on job creation in the northern portion of the study area given the propensity of businesses to locate at higher volume traffic locations. The proposed interchange will enhance economic development within the study area in Palm Beach Gardens by redirecting traffic destined for the northern portions of the study areas away from SR 786/PGA Boulevard and SR 809/Military Trail to the Central Boulevard Interchange.

Table 3-13 Employment Centers – Palm Beach County	
Employers	Employees
Palm Beach County School District	22,000
Tenet Healthcare Corp.	6,100
Palm Beach County Board of Commissioners	5,507
NextEra Energy, Inc. (Headquarters) –parent company of Florida Power & Light	3,854
HCA Palm Beach Hospitals – JFK, Palms West and West Palm	2,714
Florida Atlantic University	2,655
Bethesda Memorial Hospital	2,600
Boca Raton Regional Hospital	2,500
Veterans Health Administration	2,500
Jupiter Medical Center	2,000
Office Depot (Headquarters)	2,000
The Breakers	2,000
Florida Crystals (Headquarters)	1,700
Wells Fargo	1,367

Table 3-14 Employment Centers – Palm Beach Gardens	
Employers	Employees
G4S Secure Solutions, USA (Headquarters)	1,100
TBC Corporation (Headquarters)	807
BIOMET 3i, Inc.	471
Belcan Engineering Group	420

3.1.2.2 TAX BASE

The effect of a project on the tax base of a community may range from negligible to very significant. When considering effects on the tax base, many variables are reviewed. These variables include property values, the millage rate of a community, total ad valorem revenue collected by the community, the percentage of the budget of the community that is funded by ad valorem revenue, the percentage of the total ad valorem revenue collected in the study area, and the effect of the project on property values in the study area.

This project will not have any adverse effects on the tax base of Palm Beach County and the other adjacent municipalities. No business acquisition is required to develop the proposed improvements. The enhanced mobility has the potential to attract new businesses and support the continued growth within the tax base resulting in a long-term net economic gain.

3.1.3 LAND USE ISSUES

Land use is defined as the human use of land. Land use planning is the systematic assessment of land. This information describes how communities govern their use of land to best meet the needs of the people while safeguarding resources for the future. The congested conditions that the study area roadways experience is primarily associated with existing land use. The area has experienced rapid development over the past 20 years with associated transportation improvements trying to keep pace.

This project has been reviewed by the appropriate agencies in the ETDM Tool and has been assigned a summary degree effect of Minimal. As the project supports the land use vision depicted through the Palm Beach County Comprehensive Plan, effects on the area's character resulting from the improvement are anticipated to be minor.

3.1.3.1 LAND USE PATTERNS

The Existing Land Use Maps (see **Figure 13– Existing Land Use – Northlake Blvd. to Central Blvd.** and **Figure 14– Existing Land Use – Central Blvd. to Donald Ross Road**) show the current land use along the project corridor as mainly open space and vacant land from Donald Ross Road to Hood Road. From south of Hood Road to the end of the project limits, the area is predominantly open space and vacant land, low density residential, and recreation (Old Palm Golf Club, private) land use.

As the project improvements are to the existing I-95 facility, effects on the area's character resulting from the interchange improvements are anticipated to be minimal. The project is expected to support the growing bioscience industry and vision of the City and County, as well as the expanding residential, commercial and industrial uses in the vicinity of the interchange.

The City of Palm Beach Gardens Future Land Use Map (see **Figure 15**) dated 2011, identifies the project corridor from Donald Ross Road to Hood Road as mixed use, with a bioscience research protection overlay on the east side of I-95 (Briger Tract, see Economic section). The area from south of Hood Road to the end of the project limits is predominantly residential (low, medium and high densities) and some mixed use with bioscience research protection overlay areas.

Figure 13– Existing Land Use – Northlake Blvd. to Central Blvd.

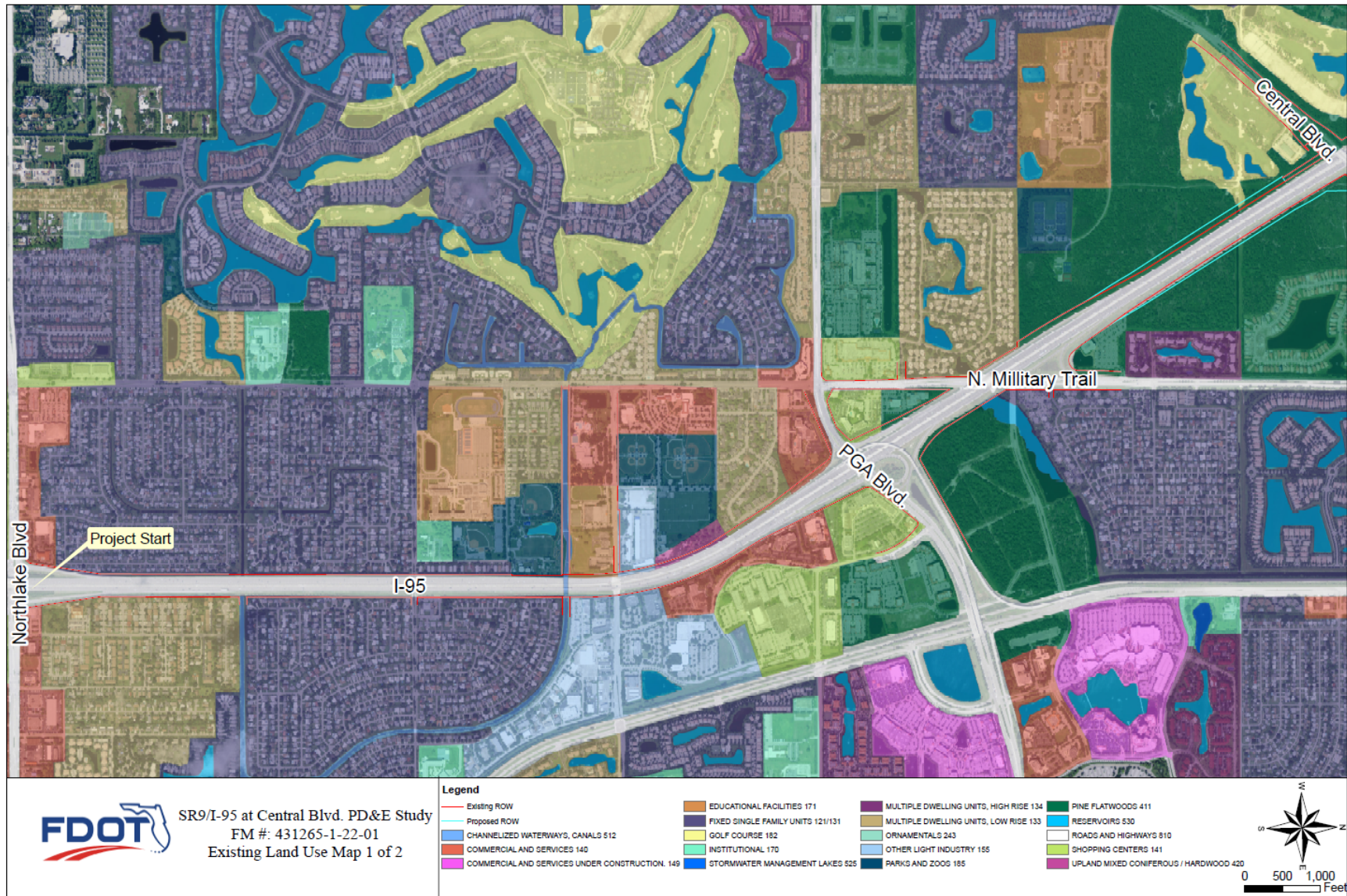


Figure 14– Existing Land Use – Central Blvd. to Donald Ross Road

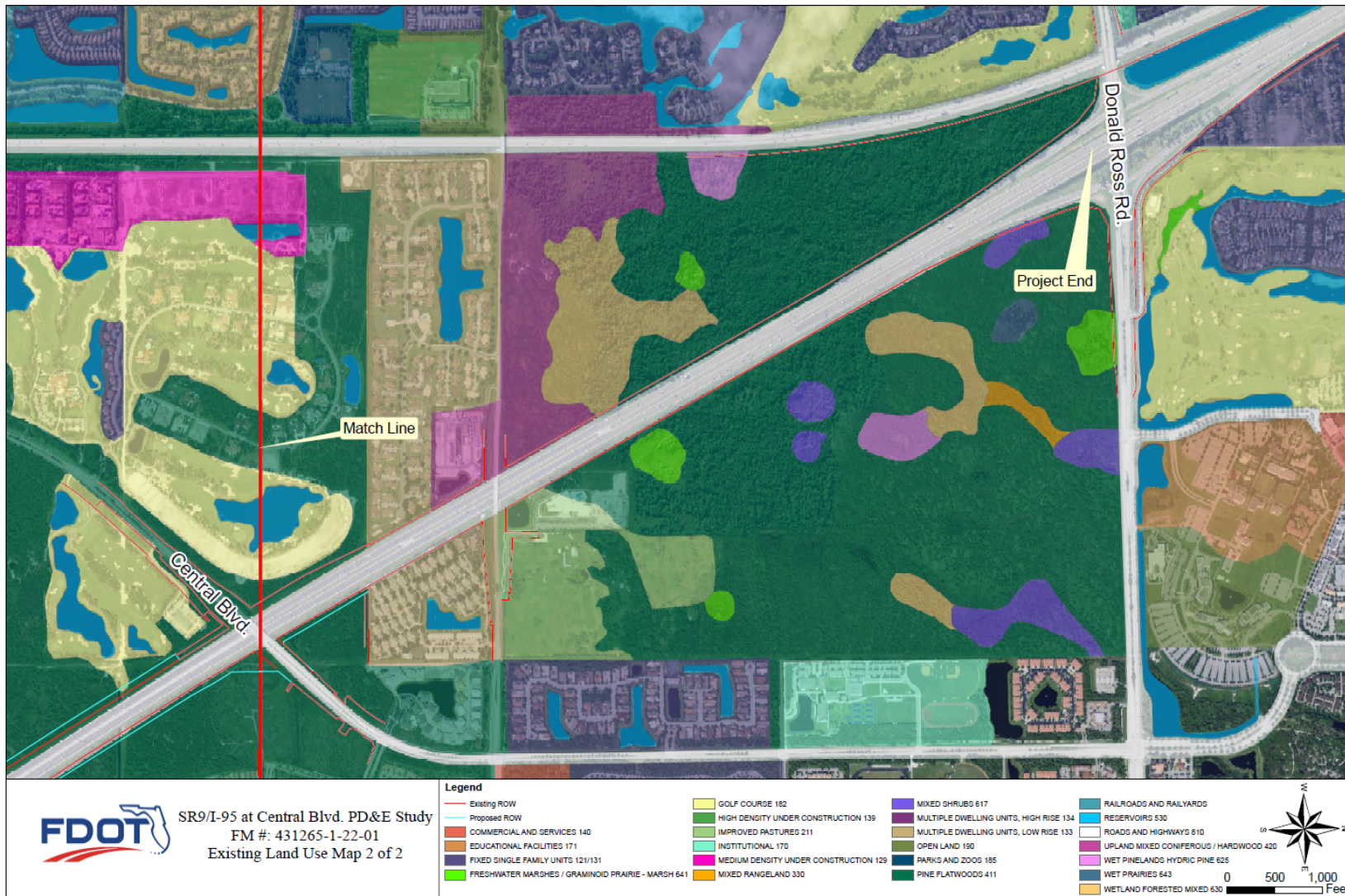
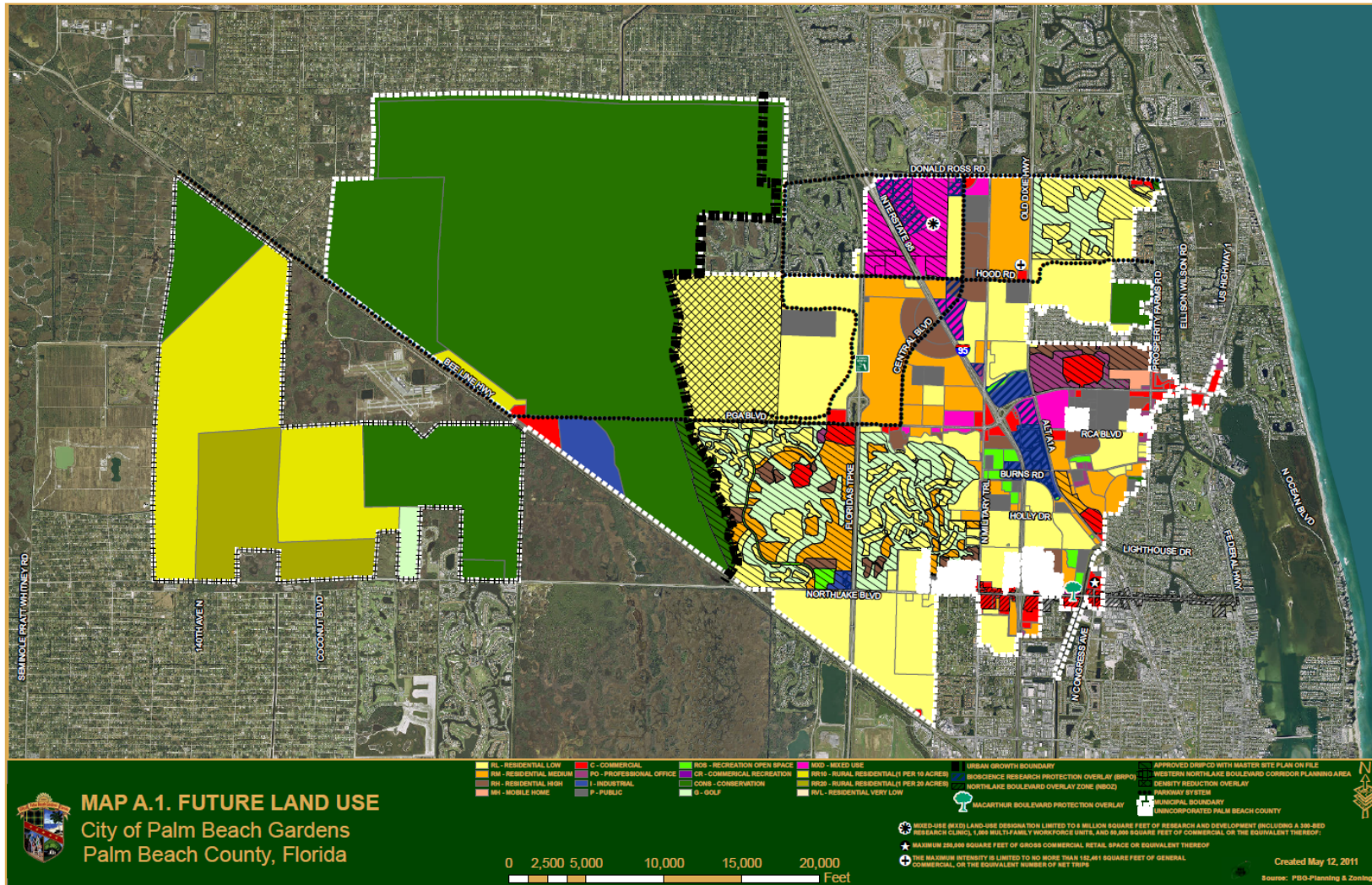


Figure 15– City of Palm Beach Gardens Future Land Use



3.1.3.2 PLAN CONSISTENCY

The Palm Beach County Metropolitan Planning Organization (MPO) 2040 Cost Feasible Plan was updated to include a new interchange at Central Boulevard. The Cost Feasible Plan was included in the MPO's Long Range Transportation Plan (LRTP), adopted in late 2014.

3.1.3.3 GROWTH TRENDS AND ISSUES

Prior to the construction of I-95 in the 1980s, most of the land in the study area was in a natural state or used for livestock grazing. After I-95 was constructed, the area gradually converted to its existing condition. The existing land use within the project area is a mix of residential and commercial uses. **Figure 14** illustrates the existing land use for the vicinity surrounding the I-95 study area. As depicted, the majority of the area is fully developed. Commercial, residential, and transportation uses are the most dominant land uses in the study area and are found consistently throughout the area east and west of the I-95 corridor.

The area surrounding I-95 is almost fully developed and future land uses will continue to reflect mixed-use development. Any changes in land use identified in the Future Land Use Plan were considered as part of the future traffic development through the transportation modeling process. The character of the study area remains relatively unchanged. The proposed interchange improvements aim to achieve acceptable Levels of Service (LOS) at the interchange in the future condition by accommodating future travel demand projected as a result of Palm Beach County population and employment growth. It will also allow I-95 to continue to serve as a critical arterial in facilitating the north-south movement of traffic in southeast Florida as it connects major employment centers, residential areas, and other regional destinations between Miami-Dade, Broward, and Palm Beach Counties. The population is expected to increase or decrease in response to regional factors unrelated to the project and it is anticipated that any future growth in the study area will be in accordance with the Palm Beach County Comprehensive Plan.

3.1.4 MOBILITY ISSUES

Mobility is the ability of people to move about freely utilizing all modes of travel including vehicular, pedestrian, bicycle, and transit. FDOT's mission statement is centered on providing a "safe transportation system that ensures the mobility of people and goods, enhances economic prosperity and preserves the quality of our environment and communities." During construction, the safety and mobility of both vehicular and pedestrian traffic must be addressed, and impacts to transit and businesses must be minimized.

This project has been reviewed by the appropriate agencies in the ETDM Tool and has been assigned a summary degree effect of Enhanced. As the I-95 project segment is one of the most heavily traveled sections of urban interstate in the nation, the Recommended Alternative will improve operational capacity and relieve local congestion and improve regional mobility.

3.1.4.1 ACCESSIBILITY

I-95 is one of the most important surface transportation facilities along the east coast of Florida, providing for the regional movement of goods and people within the twelve coastal counties, including Miami-Dade, Broward, and Palm Beach Counties. I-95 is accessible via the four existing interchanges within the study area: Northlake Boulevard, PGA Boulevard (SR 786), Military Trail (SR 809), and Donald Ross Road. Pedestrian and bicycle facilities are provided along several of the overpasses and underpasses of cross streets. Central Boulevard currently provides sidewalks along both eastbound and westbound lanes.

The project is expected to enhance access between communities and improve viable access to the major transportation facilities, businesses, and regional destinations of the area (including connectors to freight activity centers/distribution facilities). The proposed project will provide increased mobility and access to businesses to the north near the Donald Ross Road Interchange and to the residences located to the south. Additionally, the proposed project is anticipated to enhance operational capacity and relieve congestion at the existing I-95 and PGA Blvd interchange.

The sequence of construction will be planned in such a way as to minimize traffic delays. Temporary lane closures may be required and will be limited to off-peak hours, if possible. The local news media will be notified in advance of road closings and other construction-related activities. A sign providing the name, address, and telephone number of an FDOT contact person will be displayed on-site to assist the public in obtaining answers to questions or complaints about project construction. Pedestrian and bicycle facilities along the roadway crossings over or under I-95 will not be impacted.

The proposed typical sections for Central Boulevard west of I-95, including the Recommended Alternative, provide a seven-foot designated bicycle lane and a six-foot sidewalk. A seven-foot wide designated bicycle lane, and a six-foot wide sidewalk separated from the travel lanes by a pedestrian rail are provided in the westbound direction.

3.1.4.2 CONNECTIVITY

The project is located within the City of Palm Beach Gardens in Palm Beach County. The northern boundary of the study area abuts the City of Jupiter. Designated by the Florida Division of Emergency Management, I-95 serves as part of the emergency evacuation route network. I-95 is also a part of the Florida's Strategic Intermodal System (SIS).

The primary need for the I-95 and Central Boulevard interchange is to provide improved regional connectivity to the population in northern Palm Beach County. Currently there is only a single interchange serving the Palm Beach Gardens area, a densely developed suburban and urban area. The proposed interchange will help support enhanced mobility for freight by providing a more direct connection to Central Boulevard, a designated primary truck route within Palm Beach County, and providing an east-west connection to US-1 as well as several north-south arterials.

The proposed project is also anticipated to enhance emergency evacuation and response times by improving connectivity and accessibility to I-95 and other major arterials designated on the state evacuation route network, and increasing the number of residents that can be evacuated during an emergency event through expanded operational capacity.

3.1.4.3 TRAFFIC CIRCULATION

The existing Annual Average Daily Traffic (AADT) volumes for the corridor can be found in Section 2.3 Purpose and Need. The proposed interchange will enhance economic development within the study area in Palm Beach Gardens by redirecting traffic destined for the northern portions of the study areas away from SR 786/PGA Boulevard and SR 809/Military Trail to the Central Boulevard Interchange. This northern redirection of traffic will reduce congestion along PGA Boulevard and Military Trail and allow for more efficient movement of goods and people to these higher travel demand areas in the southern portion of the study area. The County's long term planning and commitment to this area is evident in the existing commercial and industrial businesses already established in the vicinity of PGA Boulevard.

3.1.4.4 PUBLIC PARKING

Development densities within the study areas are relatively low. With the exception of a few popular businesses and other community resources predominantly accessed by vehicular traffic, public parking is readily available within each development cluster. Public parking lots can be accessed from local roads. Public Parking will not be impacted by the proposed improvements.

3.1.5 AESTHETIC ISSUES

Aesthetic issues in transportation planning encompass how the community is affected visually by a project. Potential impacts include actual or perceived changes to viewsheds, exposure to noise and vibration, and compatibility of the project with the surrounding area. The placement and design of a transportation facility can diminish the aesthetic character of the surrounding area due to contrasts between natural landforms or existing structures. Roadway elements, blocked views, or a facility with a scale that is out of proportion to the surrounding landscape elements are other factors that can interfere with the aesthetic character of an area.

This project has been reviewed by the appropriate agencies in the ETDM Tool and has been assigned a summary degree effect of Minimal. Noise and vibration related effects may be of concern to proximate residences and businesses since the project traverses a residential and commercial area. However, given the presence of noise barriers and the fact that the project is consistent with future land uses identified by the Comprehensive Plans of the City of Palm Beach Gardens and Palm Beach County, impacts to aesthetics are anticipated to be minor.

3.1.5.1 NOISE AND VIBRATION ISSUES

Construction activities may generate temporary noise and vibrations that impact those businesses and residents within the immediate project vicinity. The Contractor will adhere to the measures outlined in the latest edition of the FDOT *Standard Specifications for Road and Bridge Construction*. The removal of structures and debris will be in accordance with the local and state permitting agencies.

Potential project-related impacts to noise and vibration sensitive land uses, as well as any measured proposals to abate the impacts, are addressed in the Noise Study Report prepared for the proposed project, on file at the Florida Department of Transportation, District 4.

Temporary noise impacts will occur from use of heavy equipment. In addition, construction activities may result in vibration impacts. Early identification of potential noise or vibration sensitive sites along the project corridor is important to minimize noise and vibration impacts. There are no residences, hotels, parks, and religious facilities adjacent to the proposed improvements that may be affected by noise and vibration associated with construction activities. Construction noise and vibration will be minimized by adherence to the controls listed in the latest edition of the FDOT's *Standard Specifications for Road and Bridge Construction*. According to Section 335.02 of the Florida Statutes, the FDOT is exempt from compliance with local ordinances. However, it is the FDOT's policy to follow the requirements of local ordinances to the extent that is reasonable.

A reassessment of the potential noise sensitive sites will be performed during design to ensure that impacts to such sites are minimized. Also, the contractor will be instructed to coordinate with the project engineer and the District Noise Specialist regarding any noise or vibration issues that arise during project construction.

3.1.5.2 VIEWSHED

Impacts to the existing viewshed are not anticipated to change substantially for the surrounding area as the work will occur along the existing interstate. However, construction of the new interchange could have some impacts to views from adjacent properties. Landscape trees will be affected by the construction of the new interchange ramps. However, it is anticipated that new landscaping will be included in the future design.

Construction activities for the proposed project may generate visual impacts of a temporary nature for those businesses and residents within the immediate project vicinity. The Contractor will adhere to the measures outlined in the latest edition of the FDOT *Standard Specifications for Road and Bridge Construction*.

3.1.5.3 COMPATIBILITY/VISUAL PROJECT FIT

The project involves improvements to the existing I-95 facility. The view adjacent to I-95 generally includes residential and commercial uses on either side. The project area includes transportation facilities, I-95 and Central Boulevard, that traverse through a developed urban area, with very few notable aesthetic characteristics. No unique or historic architectural features exist along the corridors in the area of the proposed project. There is one existing Palm Beach Gardens City Park located adjacent to the west edge of I-95, and one park planned for the property to the north of the Palm Beach Gardens City Park. This project is not anticipated to cause any adverse impacts to the visual/aesthetic quality or characteristics of the project corridor.

3.1.6 RELOCATION ISSUES

Approximately 11.3 acres of R/W acquisition will be necessary to accommodate the proposed improvements. This project has been reviewed by the appropriate agencies in the ETDM Tool and has been assigned a summary degree effect of Moderate. While some right of way acquisition is anticipated no residential or business relocations are expected as part of this project.

3.1.7 TITLE VI/CIVIL RIGHTS ISSUES

In accordance with the Title VI of the Civil Rights Act of 1964 and Executive Order 13166, "Improving Access to Services for Persons with LEP", the project team will make every effort to reach out to disadvantaged groups. Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Public involvement has been conducted by FDOT, with attention to Environmental Justice, to ensure transportation needs are addressed throughout the project. This outreach is detailed in the project's Public Involvement Plan.

The proposed project is not anticipated to displace any residents or businesses within the community. Increases or decreases in population are not expected as a sole result of the proposed project. The population is expected to increase or decrease in response to regional factors unrelated to the project and it is anticipated that any future growth in the study area will be in accordance with the Palm Beach County Comprehensive Plan. Changes to social relationships and patterns and disruptions to community cohesion are not anticipated since the proposed project is located within an existing I-95 facility. The proposed project will provide increased mobility and access to businesses to the north near the Donald Ross Road Interchange and to the residences located to the south.

3.1.7.1 NON-DISCRIMINATION CONSIDERATION

Demographic information obtained for the 2015 IJR study area indicate the presence of low income and minority populations living along the project corridor. However, it should be noted that within the PD&E Study area, low-income populations are not present. A review

of the potential impacts to demographics, community cohesion, safety and community goals, and quality of life issues was conducted. While there is presence of low income and minority populations, impacts are expected to be minimal.

3.1.7.2 CONTROVERSY POTENTIAL

The proposed improvements for the Recommended Alternative are not anticipated to require relocations. Substantial controversy was not identified during the public outreach activities conducted during the study.

4.0 PUBLIC INVOLVEMENT

Throughout the course of this study, there has been continuous and broad-based public involvement. Public outreach activities have included small group meetings and workshops conducted with property owners, and large scale general public meetings and agency coordination. Public outreach tools included newsletters and a project website. This section provides a summary of the coordination efforts and comments received from the community throughout the study. Please see **Appendix B** for a summary of public comments received.

4.1 ADVANCED NOTIFICATION

Advanced Notification (AN) is the means through which Federal, State and local agencies are informed of proposed actions by FDOT. It gives notice of FDOT's intent to apply for federal aid for a project. The AN provides an opportunity for early involvement of federal, state, and local agencies in the PD&E phase and allows agencies to share information and concerns regarding the proposed action. In 2012, FDOT mailed a project AN package to the Florida Department of Environmental Protection Florida State Clearinghouse. FDOT then distributed the package to federal, state and local agencies, as well as appropriate government representatives. A copy of the AN and responses received from the agencies is included in the ETDM Summary Report provided in **Appendix C**.

4.2 PUBLIC INVOLVEMENT PROGRAM (PIP)

A project specific Public Involvement Plan (PIP) was developed at the beginning of the study process. The PIP was created to guide the project team's communications and interactions with all interested parties during the study. The plan describes specific methods and techniques regarding the public involvement approach for the project and ensures a free flow of information between the FDOT, property owners, local governments, agencies, stakeholder groups, business owners, and other interested parties.

4.3 PUBLIC MEETINGS

A summary of public meetings held to date during the study is provided below.

Kick-Off Meeting

A Kick-off meeting was held on Thursday, January 29, 2015 at the City of Palm Beach Gardens Council Chamber, located at 10500 North Military Trail, Palm Beach Gardens, Florida. The agency Kick-off meeting started in an open house format at 2:30, with a presentation at 3:00 pm. The Public Kick-off meeting included time for attendees to review exhibits and talk with project team representatives in an open house format from 5:30 to 6:00 pm. A formal presentation was made at 6:00 pm. The purpose of these meetings was to provide elected officials, residents, businesses, and interested parties an opportunity to ask questions, provide comments and obtain information about the study.

Approximately 30 individuals attended the Agency Kick-off, representing local agencies, elected officials, and media, as well as FDOT staff and consultants. The Public Kick-off was attended by approximately 125 residents, business owners, interested parties, members of the media and staff. The two media outlets in attendance were the Palm Beach Post and ABC affiliate WBPF 25.

Alternatives Public Workshop

An Alternatives Public Workshop was held on Thursday, February 18, 2016 at the City of Palm Beach Gardens Council Chamber, located at 10500 North Military Trail, Palm Beach Gardens, Florida 33410. The format of the workshop was an open house. Graphics and informational displays were available for review from 5:30 pm to 7:30 pm. The purpose of the workshop was to provide elected officials, residents, businesses, stakeholders and interested parties an opportunity to provide input concerning the alternatives under consideration and to encourage interested parties to submit their comments. Comment forms were available. The Workshop was attended by over 100 residents, business owners, interested parties, and staff. A handout was provided with information about the alternatives under consideration.

Twenty-six written comments were received in response to the workshop. These included comments submitted on comment forms (either left at the workshop or mailed to the project manager after the workshop), and emails sent to the project manager. Some individuals indicated a preference for a specific alternative, or opposed specific alternatives. Of these, 15 opposed construction of any new interchange, regardless of the configuration. Four individuals stated they preferred construction of a new tight diamond urban interchange over construction of a diverging diamond interchange. One individual expressed a preference for the CD road system over the braided ramp system.

Public Hearing

The Public Hearing was held on Wednesday, September 28, 2016 at the City of Palm Beach Gardens Council Chamber, located at 10500 North Military Trail, Palm Beach Gardens, Florida 33410. An open house began at 5:30 pm. Graphics and informational displays were available for review from 5:30 p.m. to 7:30 p.m. The presentation began at 6 p.m., outlining the process by which the Recommended Alternative was selected, and summarizing its environmental impacts. A comment period followed, that provided elected officials, residents, businesses, stakeholders and interested parties an opportunity to provide input concerning the Recommended Alternative. Comment forms were available. Attendees could also dictate their comment to the reporter recording the hearing. The Public Hearing was attended by nearly 100 residents, business owners, interested parties and staff. A handout provided information about the Recommended Alternative.

Twenty citizens and elected officials provided comments in response to the Public Hearing (before, during, and after). These included comments submitted following the notice of the Hearing, provided verbally during the Hearing, submitted on comment forms (either left at the Hearing or mailed to the project manager after the Hearing), dictated to the reporter, or emails sent to the project manager. Of these, nine people opposed construction of a new interchange at Central Boulevard.

4.4 OTHER MEETINGS

The study team met with City of Palm Beach Gardens, Palm Beach County MPO, and Palm Beach County School Board staff on November 10, 2015 at the MPO office, 2300 North Jog Road, West Palm Beach, Florida and presented an update on the study progress.

A presentation to Palm Beach County District One Commissioner Hal Valeche on January 20, 2016 at his office at 3188 PGA Boulevard, Palm Beach Gardens, Florida.

The project team gave a presentation to the Northern Palm Beach County Chamber of Commerce Government Affairs Committee to update them on the progress of the PD&E study. The presentation was made on Friday, February 12, 2016 at the North Palm Beach Country Club, 951 US Hwy 1, North Palm Beach, Florida.

A presentation was made to the Palm Beach Gardens City Council to update them on the progress of the study. The presentation was made on Thursday, April 7, 2016 at the City of Palm Beach Gardens Council Chamber, located at 10500 North Military Trail, Palm Beach Gardens, Florida.

4.5 PROJECT WEBSITE

A project specific FDOT web page was established (www.95pgacentralblvd.com) to provide updated information about the project and upcoming public meetings.

Website contents include:

- Project Fact Sheet;
- Project Schedule;
- Kick-off meeting presentation;
- Alternatives Workshop Display Boards;
- Northern Palm Beach Chamber of Commerce Government Affairs Committee presentation; and
- Interchange Justification Report.

5.0 CONCLUSIONS AND RECOMMENDATIONS FOR RESOLVING ISSUES

Review and consideration of all potential sociocultural effects that could result from construction of the proposed improvements indicate that the project will have minimal or no impacts to the communities within the project area. Anticipated improvements to mobility within the project corridor, and regional mobility will potentially benefit the community. Based on noise contours generated for this project, minor noise impacts could occur to the residences located immediately adjacent to I-95. Approximately 1.3 acres of future Palm Beach County District Park, located immediately adjacent to I-95, will be acquired. The proposed new tight-diamond interchange at Central Boulevard may have minor visual impacts to adjacent land owners, however, visual impacts are anticipated to be minimal.

5.1 RECOMMENDATIONS FOR RESOLVING ISSUES

A noise study report was prepared to evaluate potential noise impacts to adjacent properties from construction of the Recommended Alternative. Noise barriers were evaluated as an abatement measure for the 152 residential and six recreational noise sensitive receptors predicted to experience future build traffic noise levels that approach, meet, or exceed the Noise Abatement Criteria (NAC) for their respective activity category of the NAC. Based on the results of the analysis, noise barriers are a potentially feasible and cost reasonable noise abatement measure for up to 94 of the impacted receptors located in Garden Lakes (Noise Barrier 1), Winchester Court (Noise Barrier 5) and the Quay Apartments (Noise Barrier 8). Efforts will be made in the Design Phase of this project to further minimize noise impacts and acquisition of adjacent property. Visual impacts are subjective and unavoidable, but are anticipated to be minimal.

Palm Beach Gardens City Park and the planned Palm Beach County District Park are located adjacent to the west edge of I-95. Potential impacts to these resources are documented in the Section 4(f) Determination of Applicability and Section 4(f) *de minimis* Request Package prepared for this project and will be minimized to the extent practicable during the design process. Although approximately 1.33 acres will be acquired from the District Park, these impacts are anticipated to be minimal. Since the area impacted is located immediately adjacent to I-95, Palm Beach County does not have a final development plan for the park. Accordingly, the park can be designed to minimize the impact.

5.2 PROJECT COMMITMENTS

The FDOT will make the following commitments related to Socio-cultural issues:

- The FDOT is committed to the construction of feasible noise abatement measures at the locations where noise barriers have been recommended for further consideration during the final design phase, contingent upon the following conditions:
- Detailed noise analyses during the final design process support the need for abatement;
- Reasonable cost analyses indicate that the economic cost of the barrier(s) will not exceed the cost reasonable criterion;
- Safety and engineering aspects as related to the roadway user and the adjacent property owner have been reviewed and any conflicts or issues resolved;
- Community input regarding desires, types, heights and locations of barriers has been solicited by the FDOT; and
- Any other mitigating circumstances found in Section 17-4.6.1 of FDOT's PD&E Manual have been analyzed.
- A reassessment of the project corridor for additional sites particularly sensitive to construction noise or vibration will be performed during design to ensure that impacts to such sites are minimized. Coordination between the FDOT and the operators of any construction-noise or vibration sensitive locations identified during design will occur, and if applicable, special provisions developed for the project's contract package in order to ensure that impacts to such businesses are minimized.
- The FDOT will reevaluate the feasibility and reasonableness of noise abatement measures during Final Design if warranted by changes to the project's design.
- Construction noise and vibration impacts will be minimized by adherence to the controls listed in the latest edition of the FDOT's Standard Specifications for Road and Bridge Construction.
- The sequence of construction will be planned in such a way as to minimize traffic delays. The project will involve the development and use of a

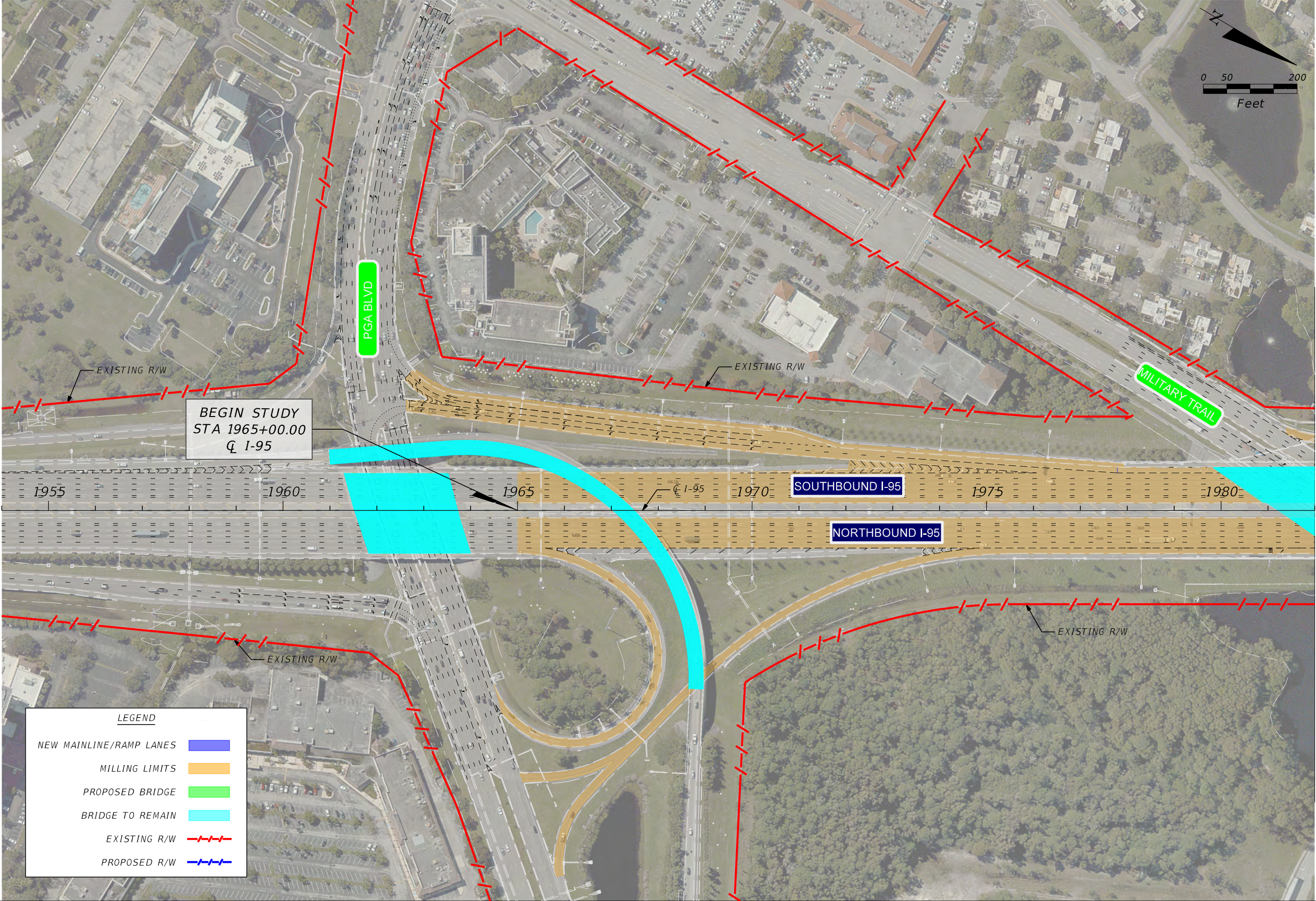


Maintenance of Traffic (MOT) Plan. This Plan will include traffic management and signage, access to local businesses and residences, detour routes, public notification of alternate routes, emergency services coordination and project scheduling. The local news media will be notified in advance of road closings and other construction-related activities which could excessively inconvenience the community so that business owners, residents, and tourists in the area can plan travel routes in advance. A sign providing the name, address, and telephone of an FDOT contact person will be displayed on-site to assist the public in obtaining answers to questions or complaints about project construction.



APPENDIX A

Alternative Concept Plans



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LEGEND

NEW MAINLINE/RAMP LANES

MILLING LIMITS

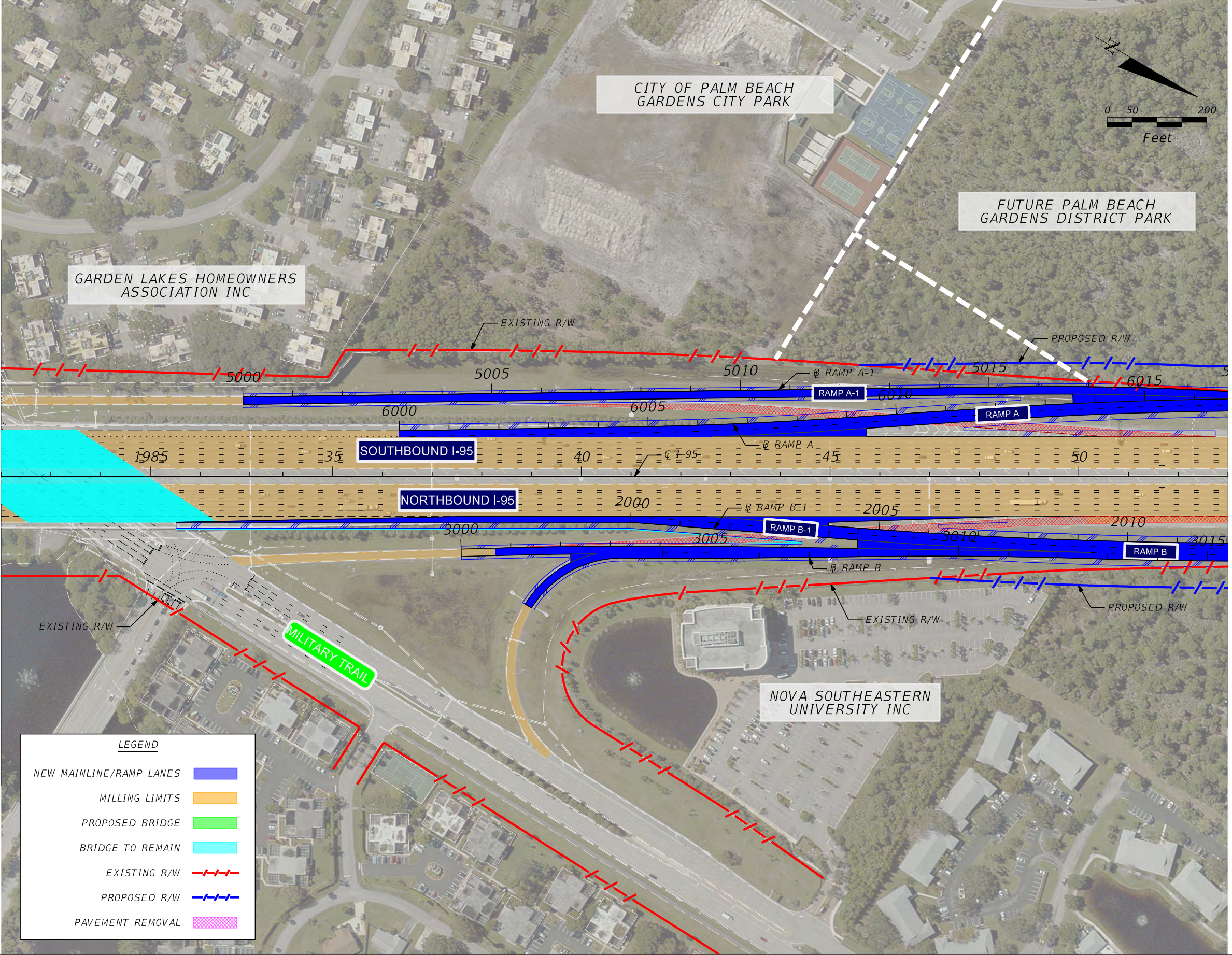
PROPOSED BRIDGE

BRIDGE TO REMAIN

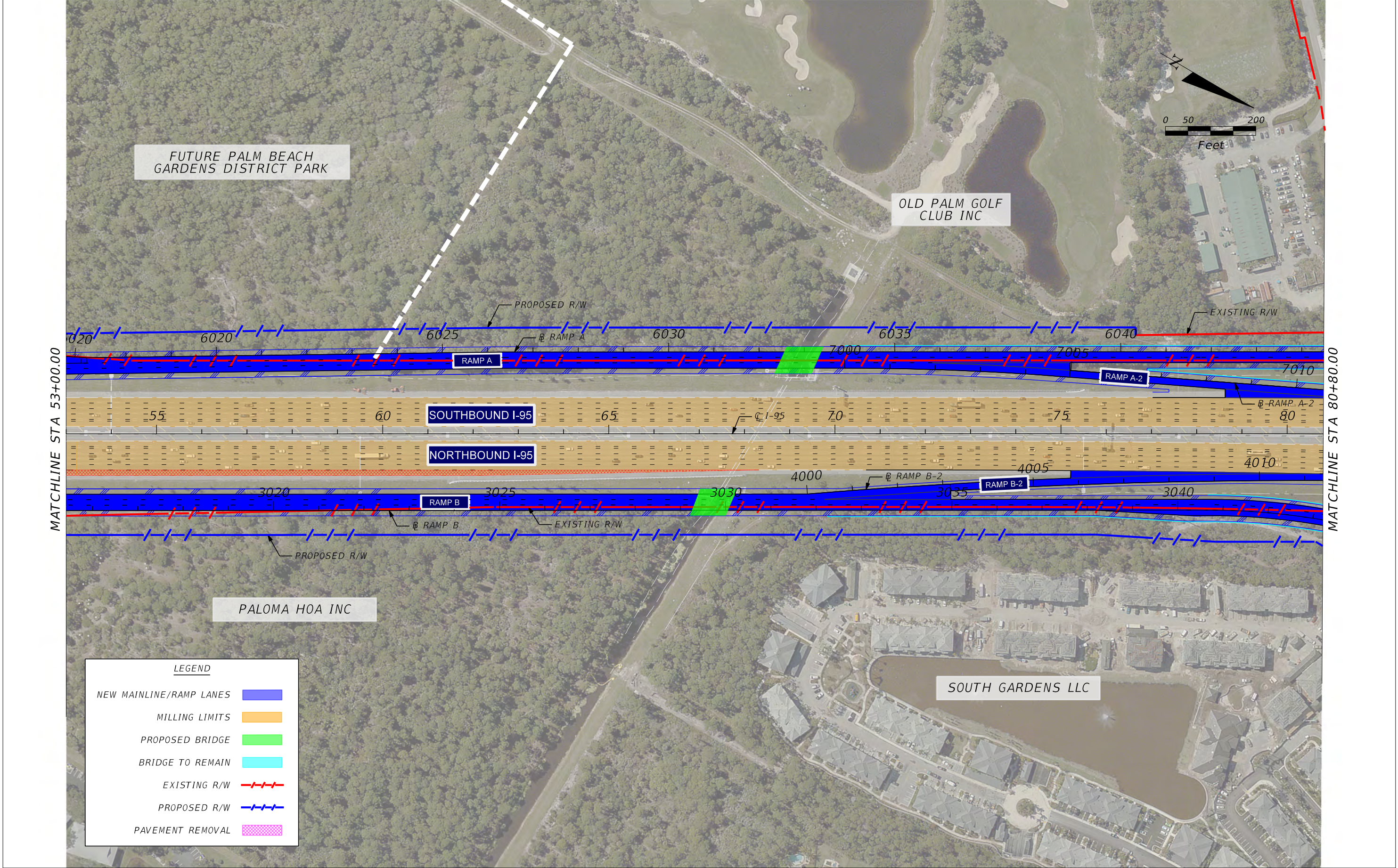
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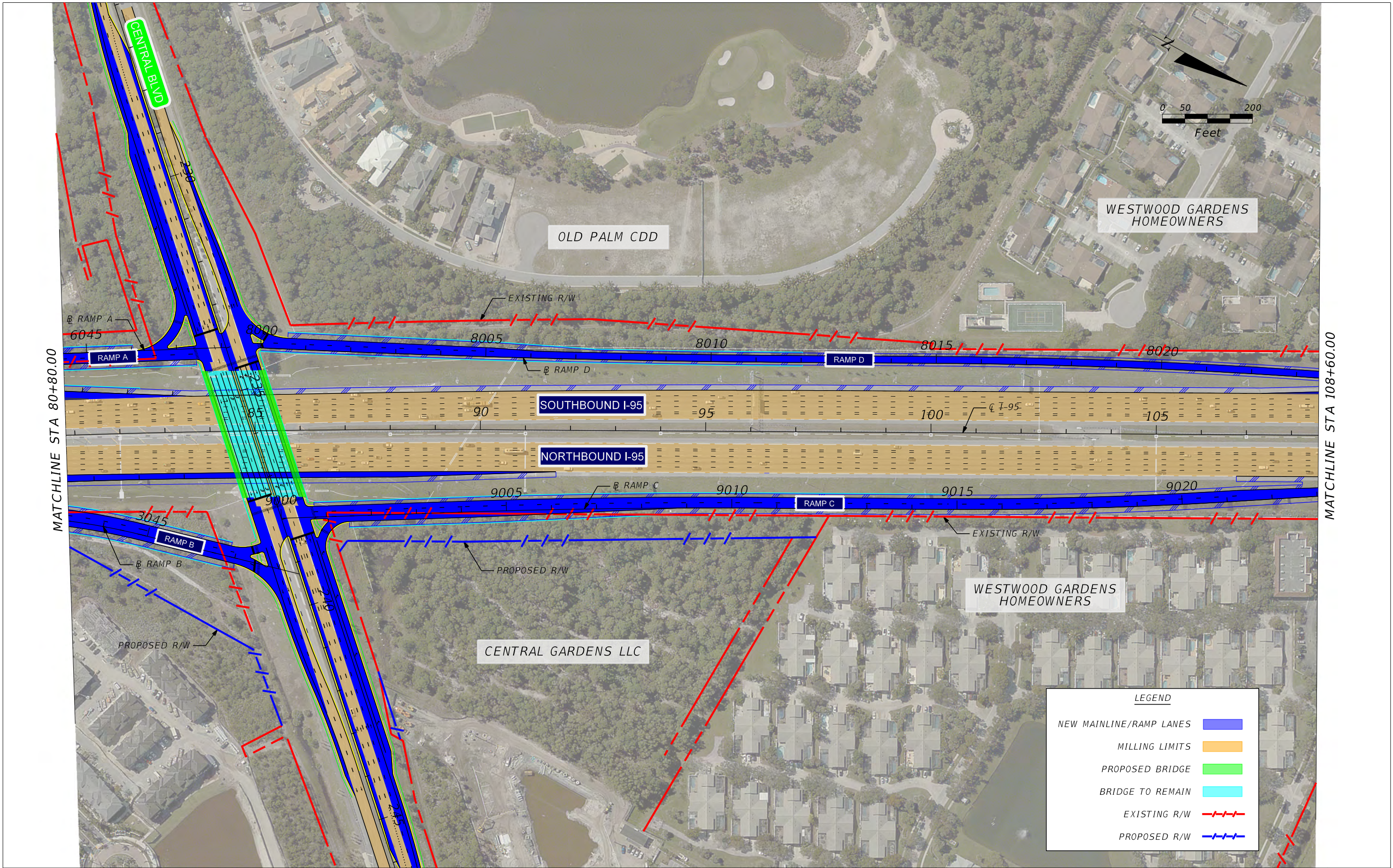
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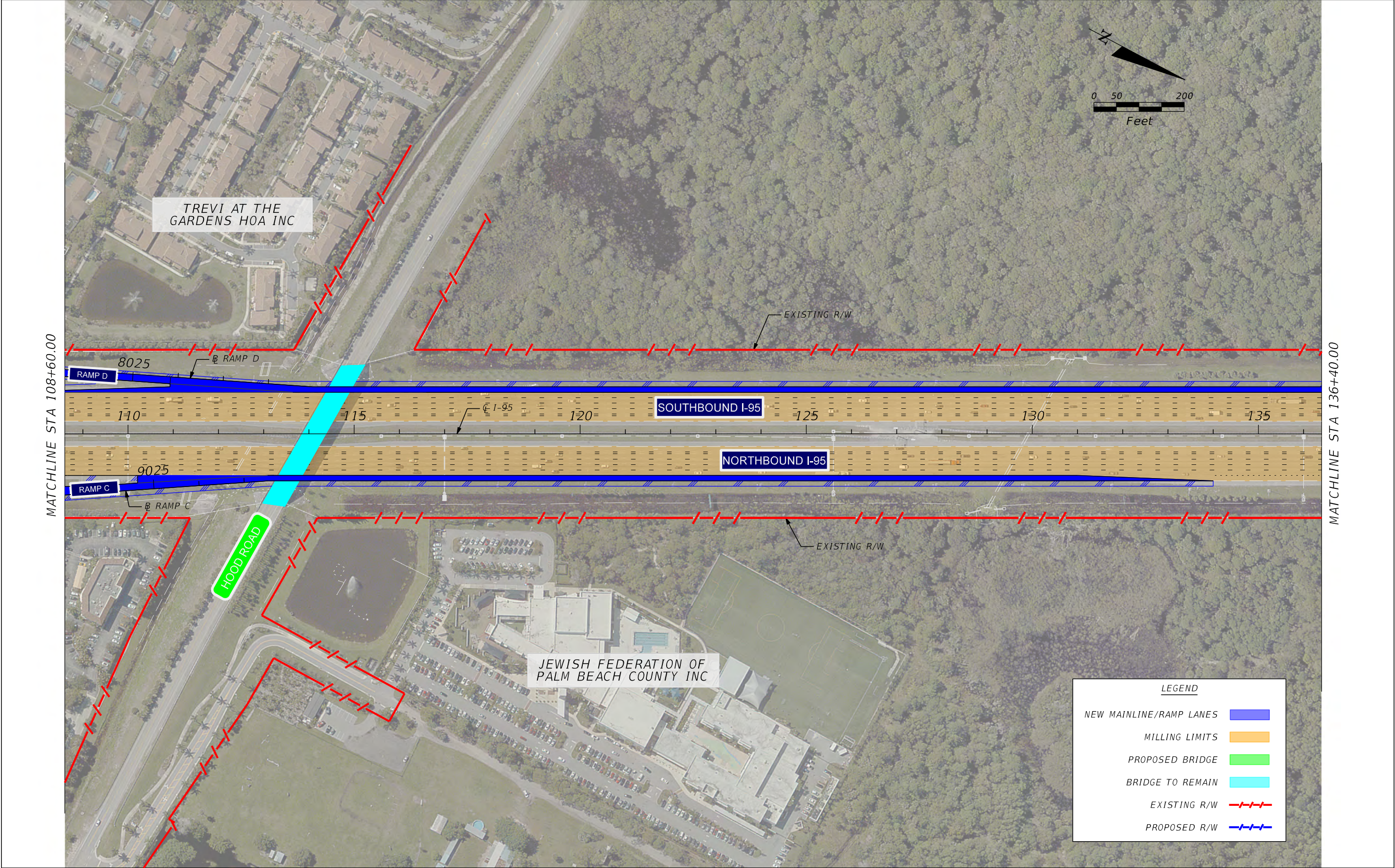
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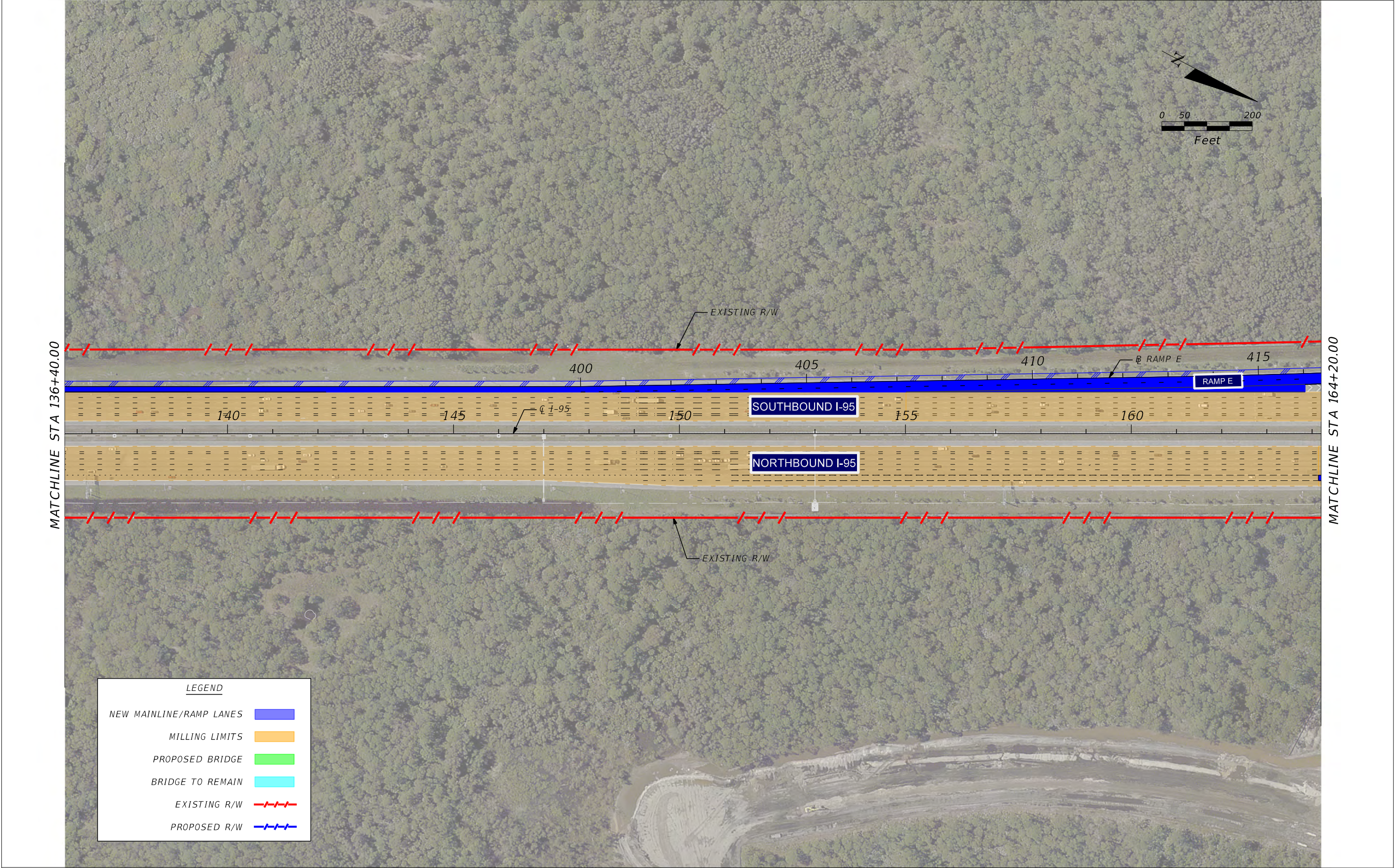
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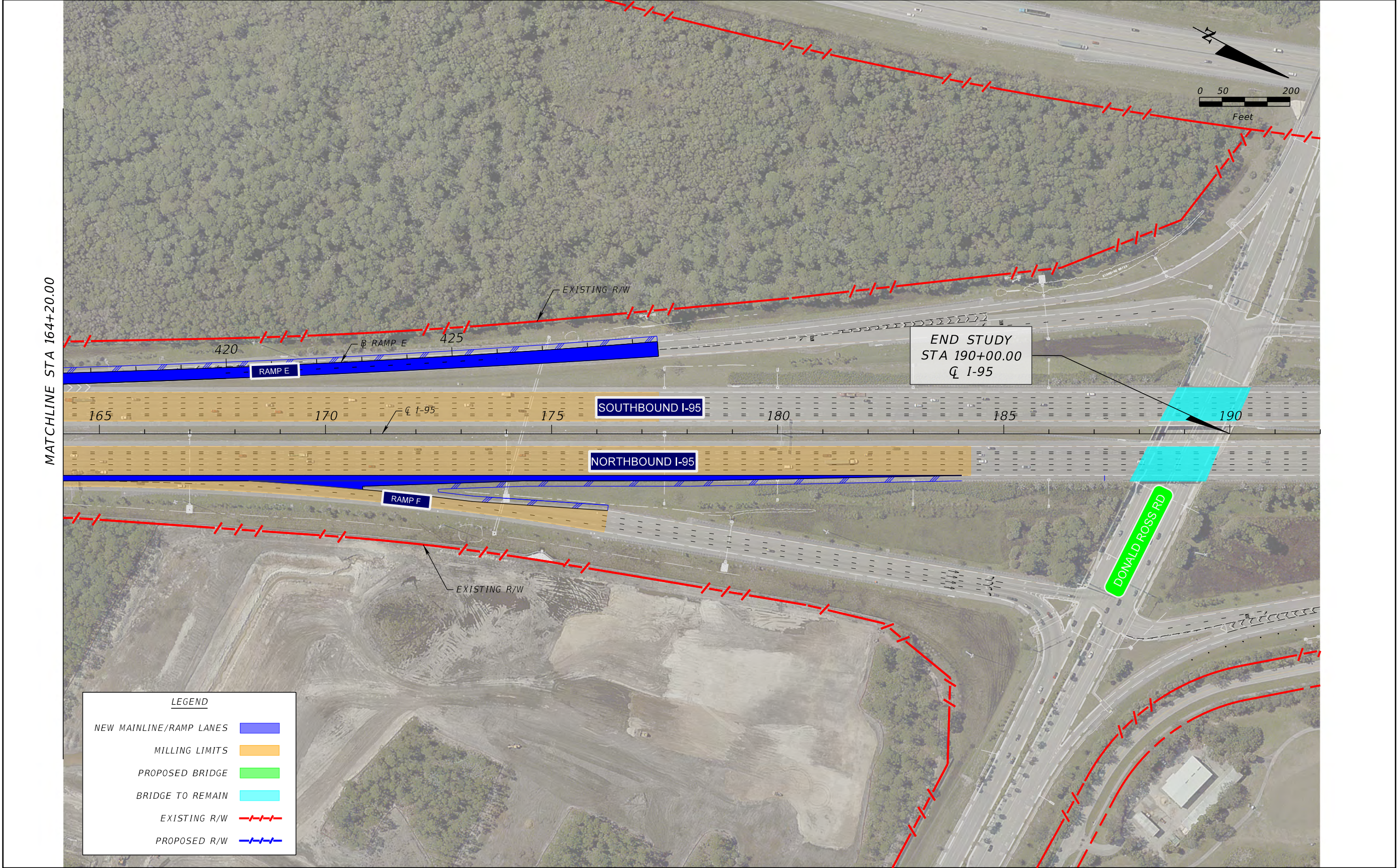
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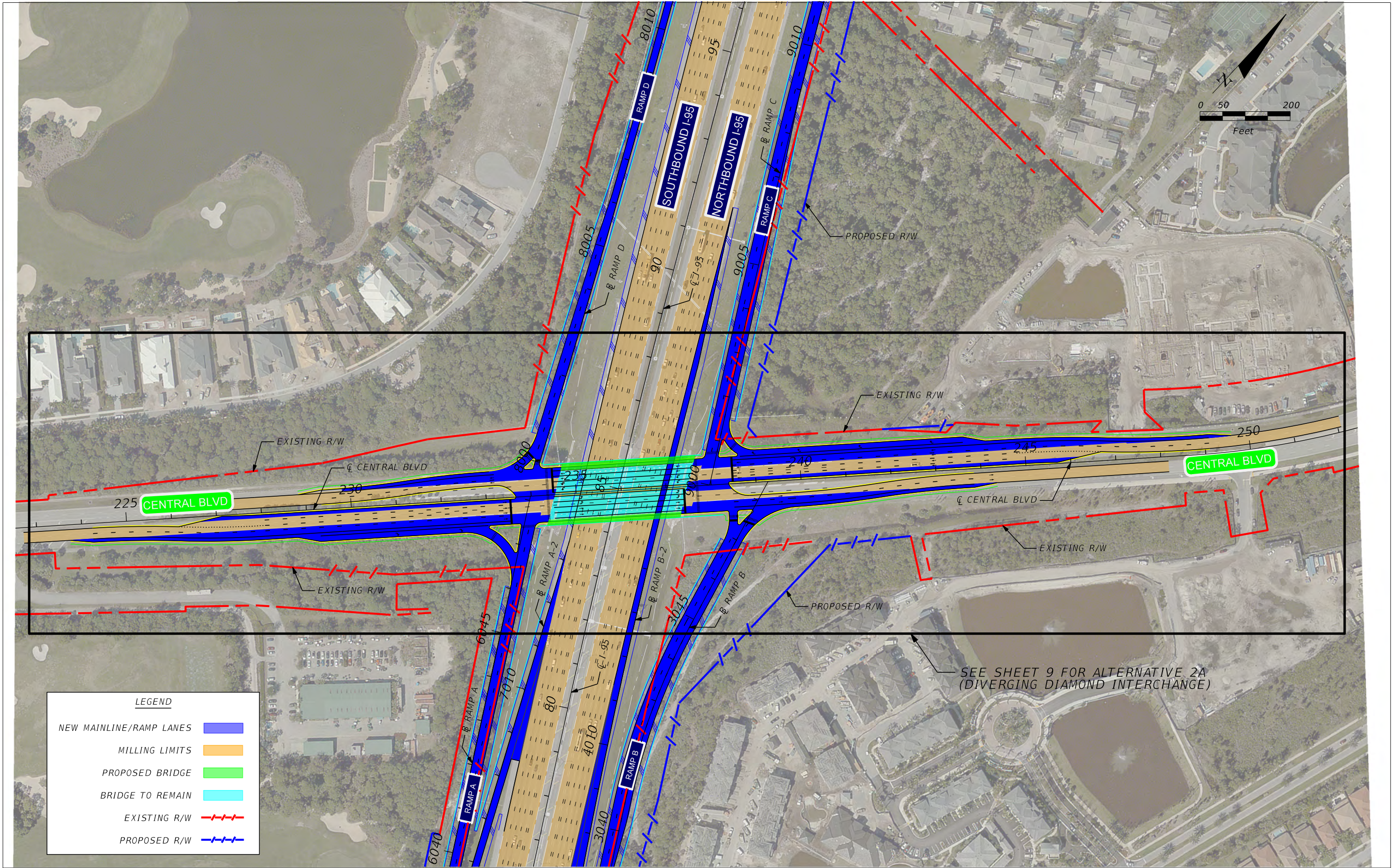
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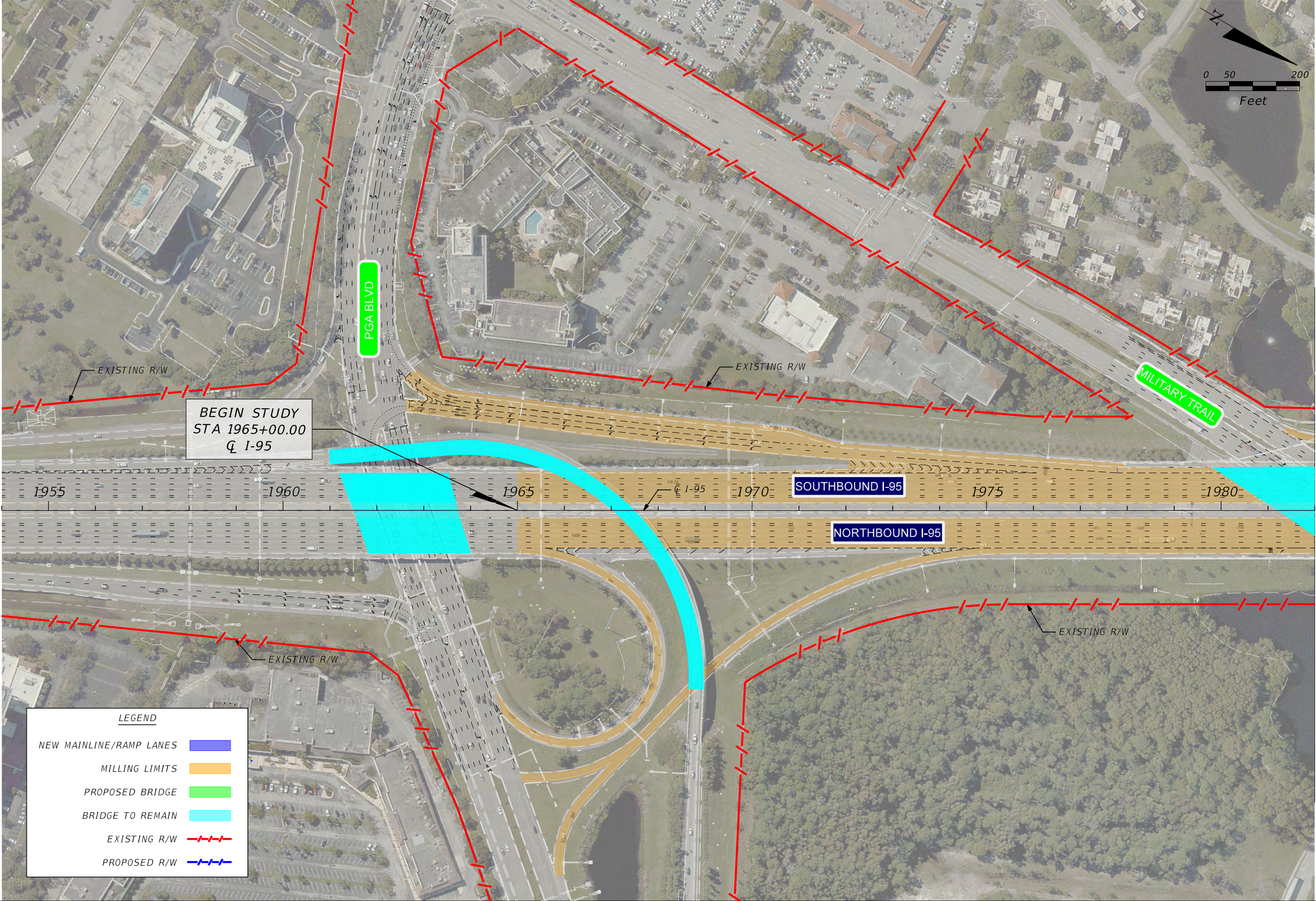


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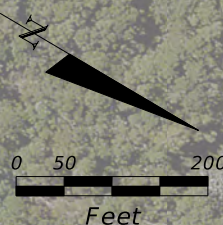
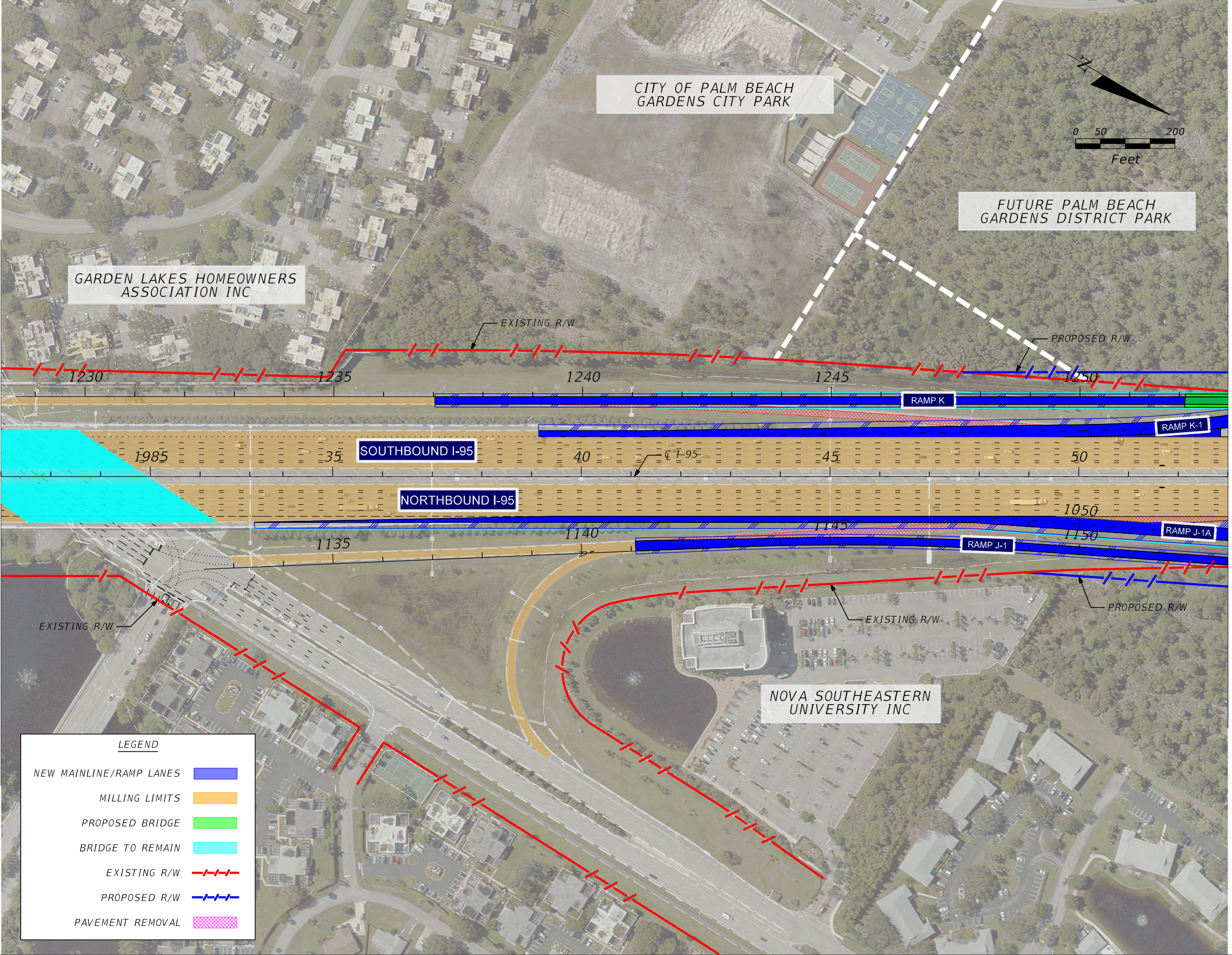
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I-95 FROM PGA BLVD. TO
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NEW MAINLINE/RAMP LANES	
MILLING LIMITS	
PROPOSED BRIDGE	
BRIDGE TO REMAIN	
EXISTING R/W	
PROPOSED R/W	
PAVEMENT REMOVAL	

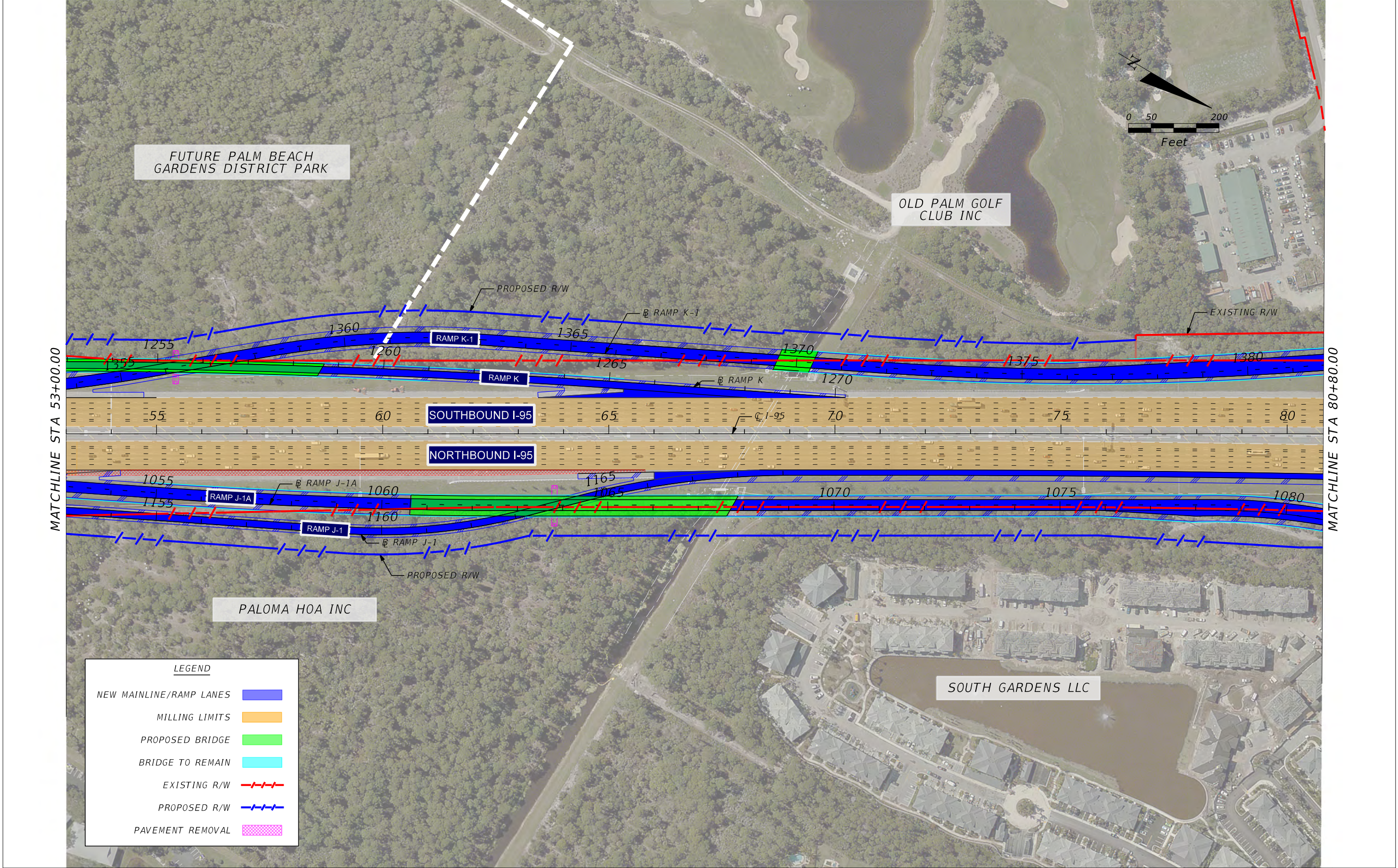
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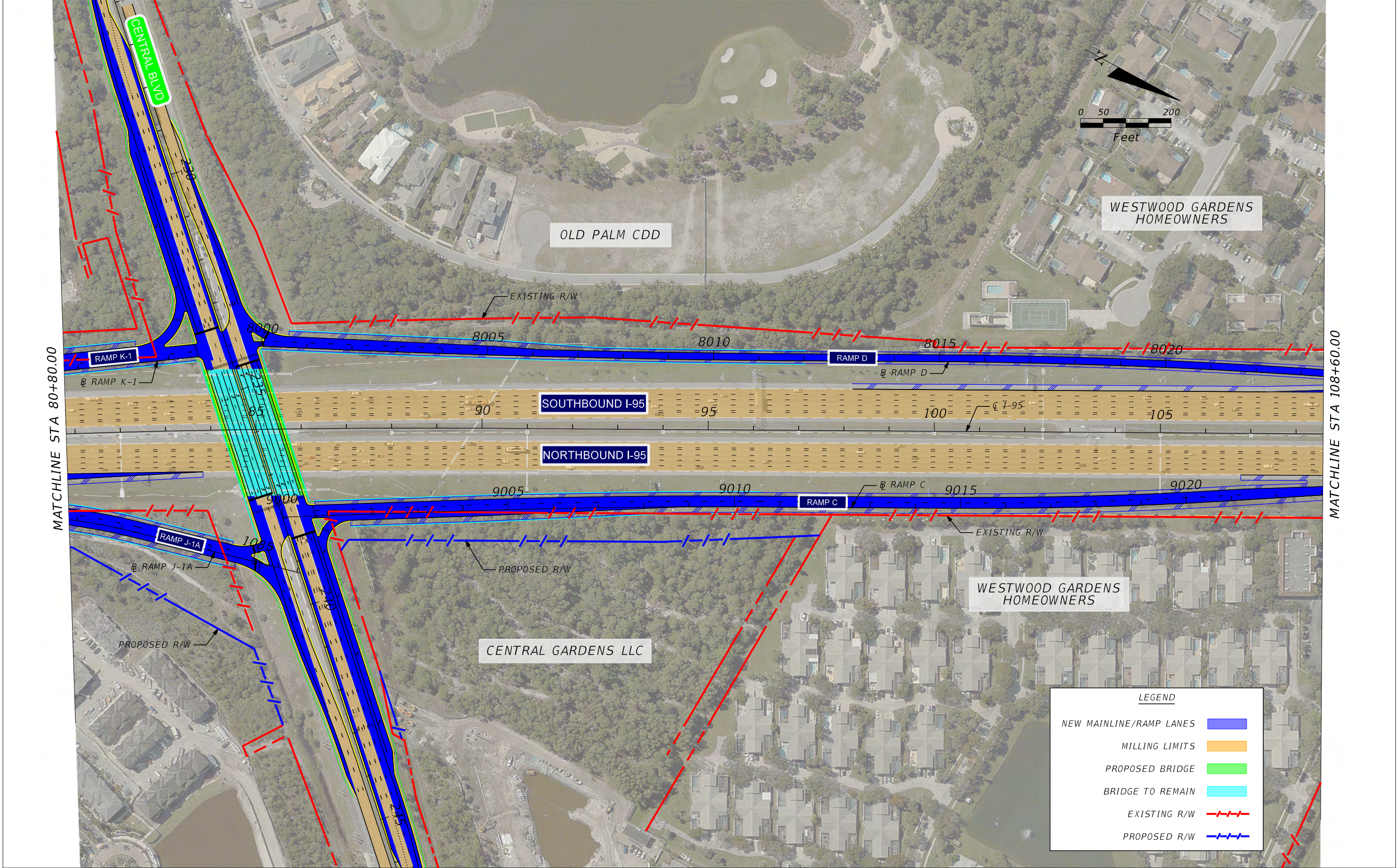
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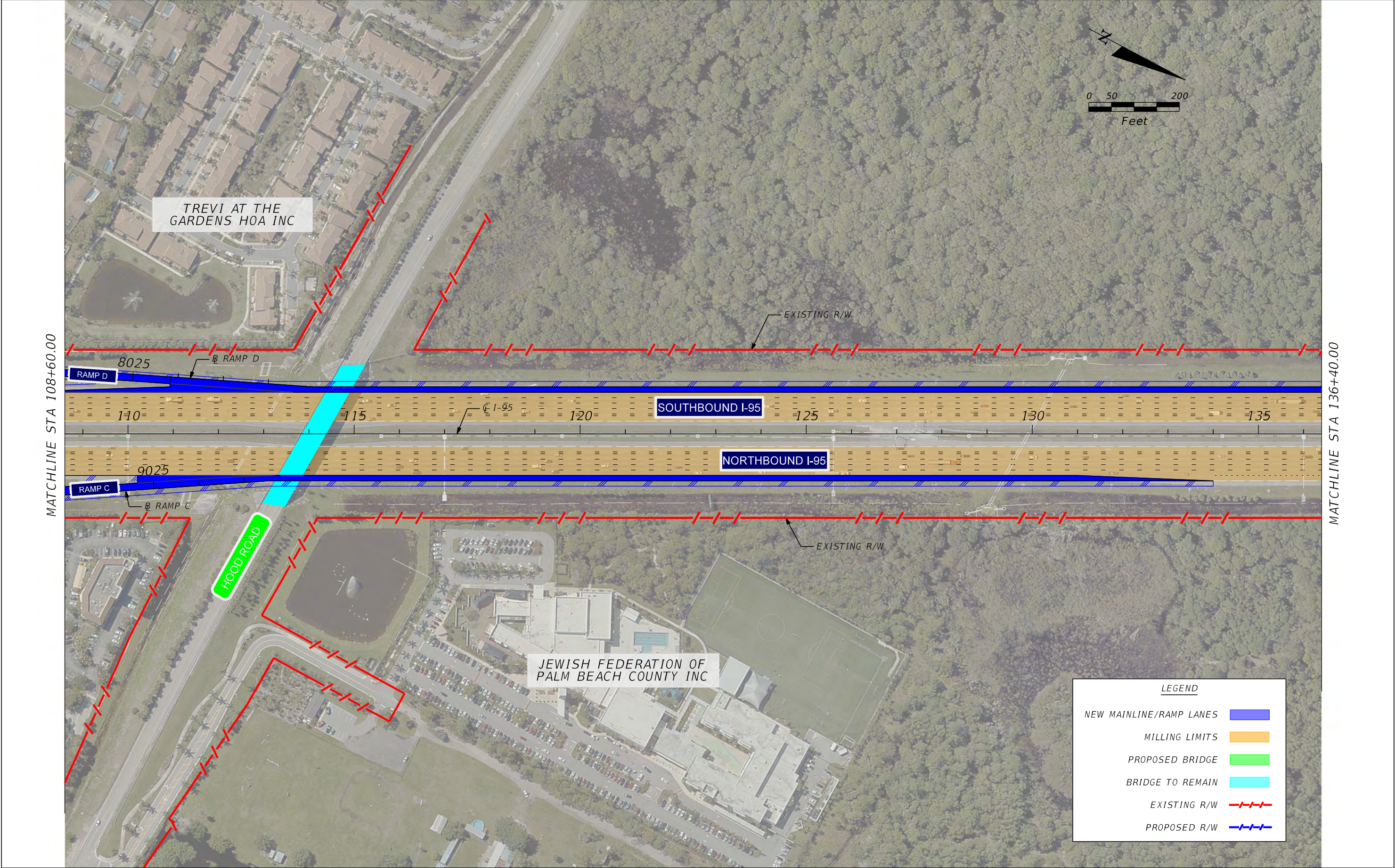
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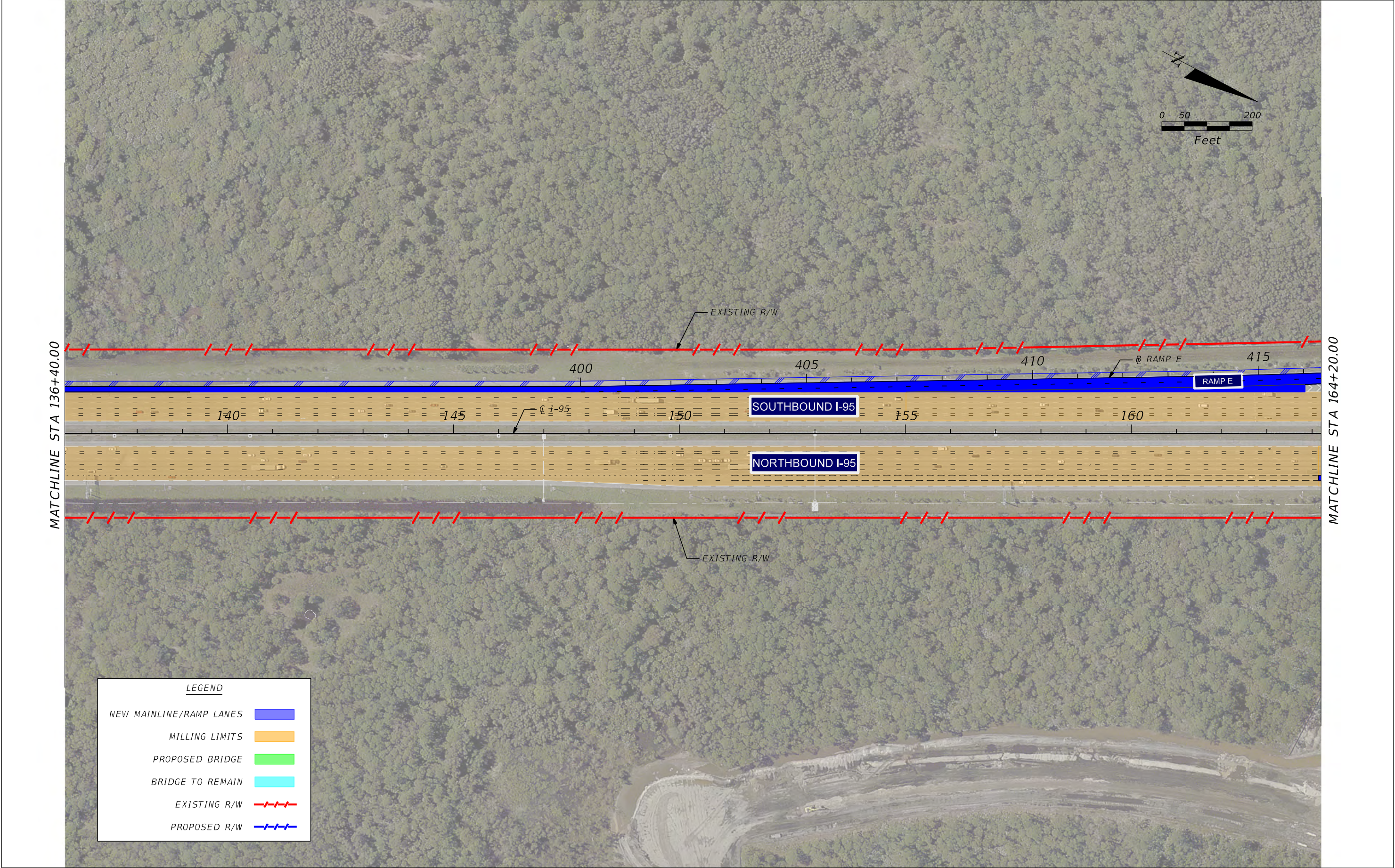
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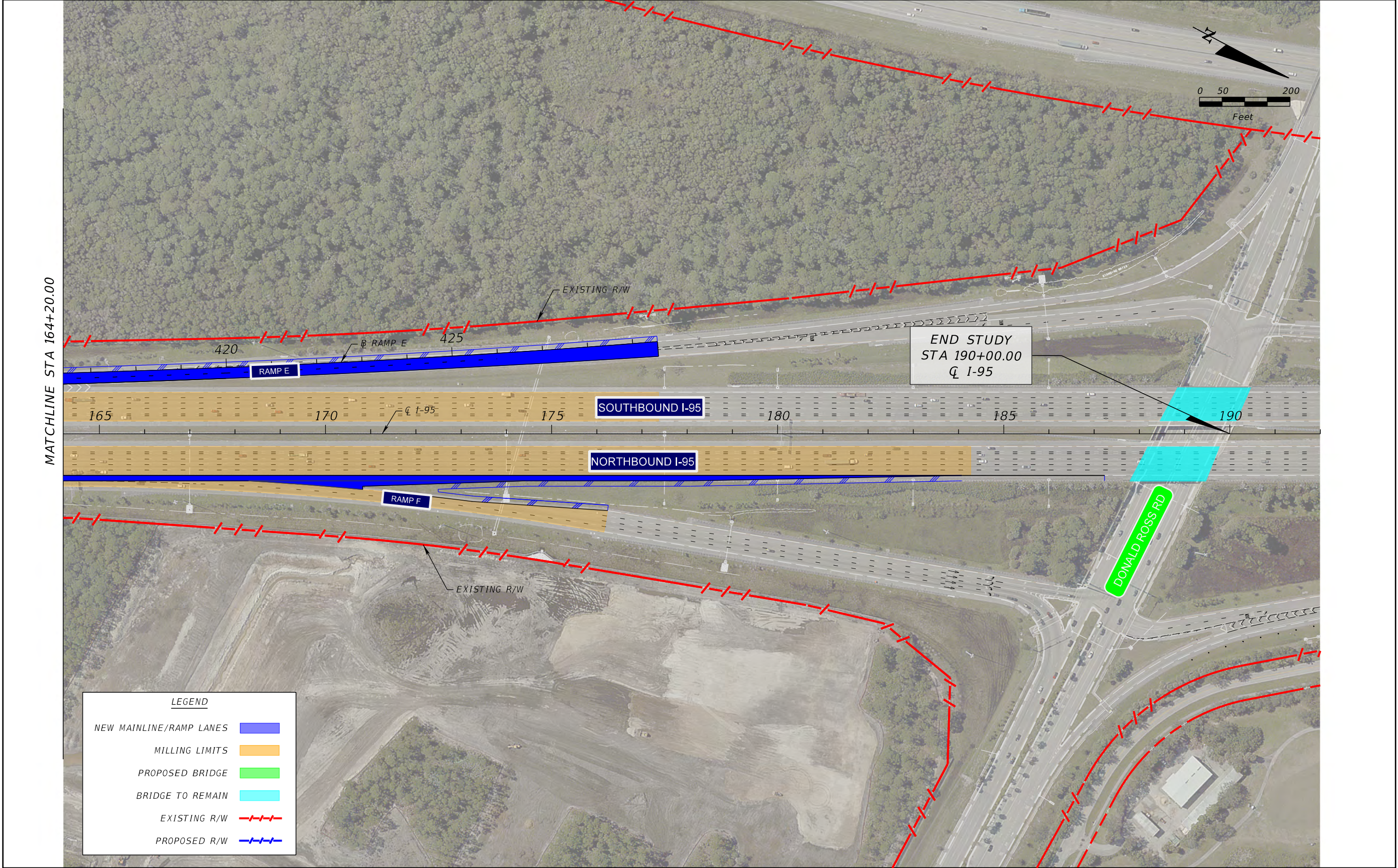
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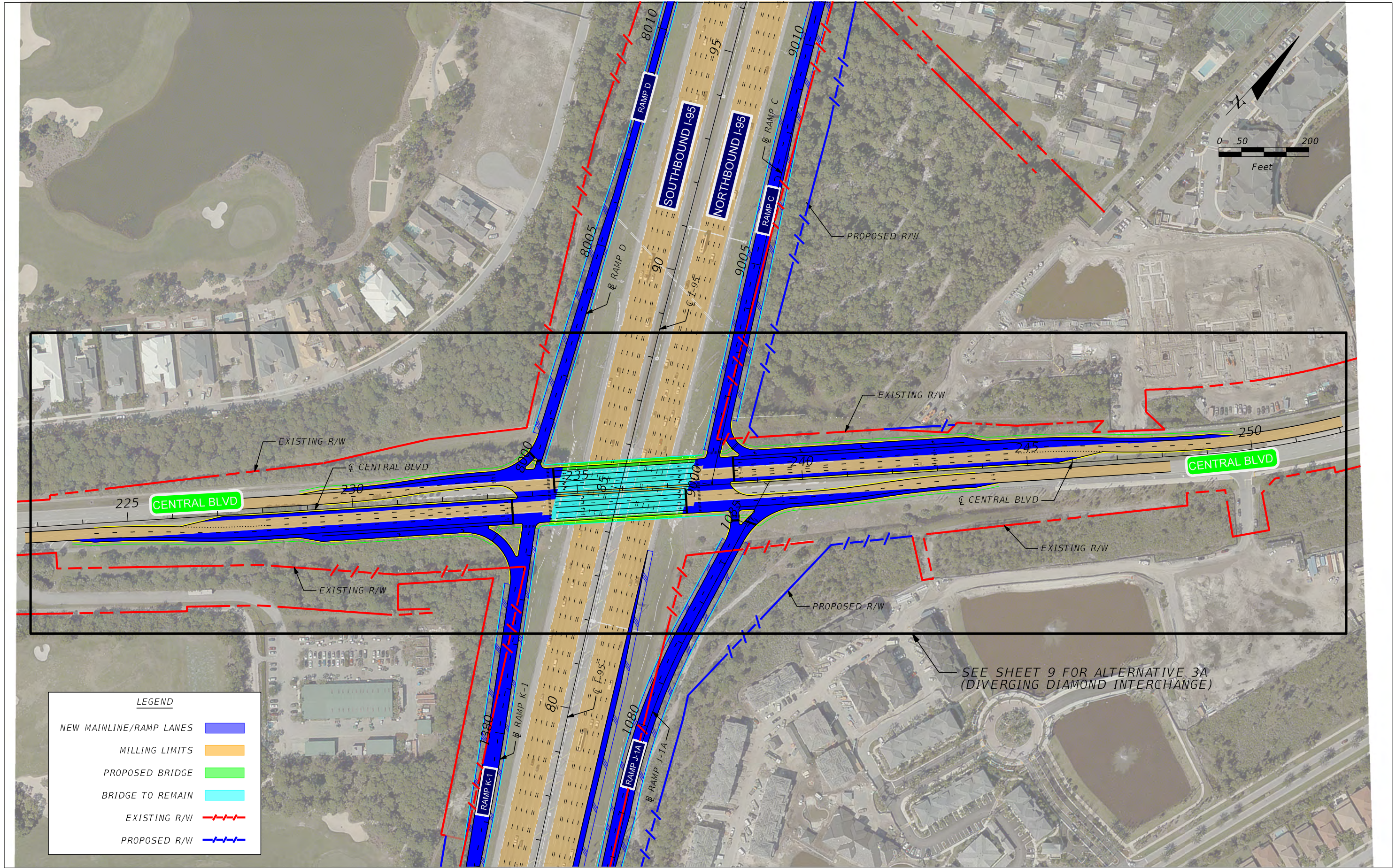
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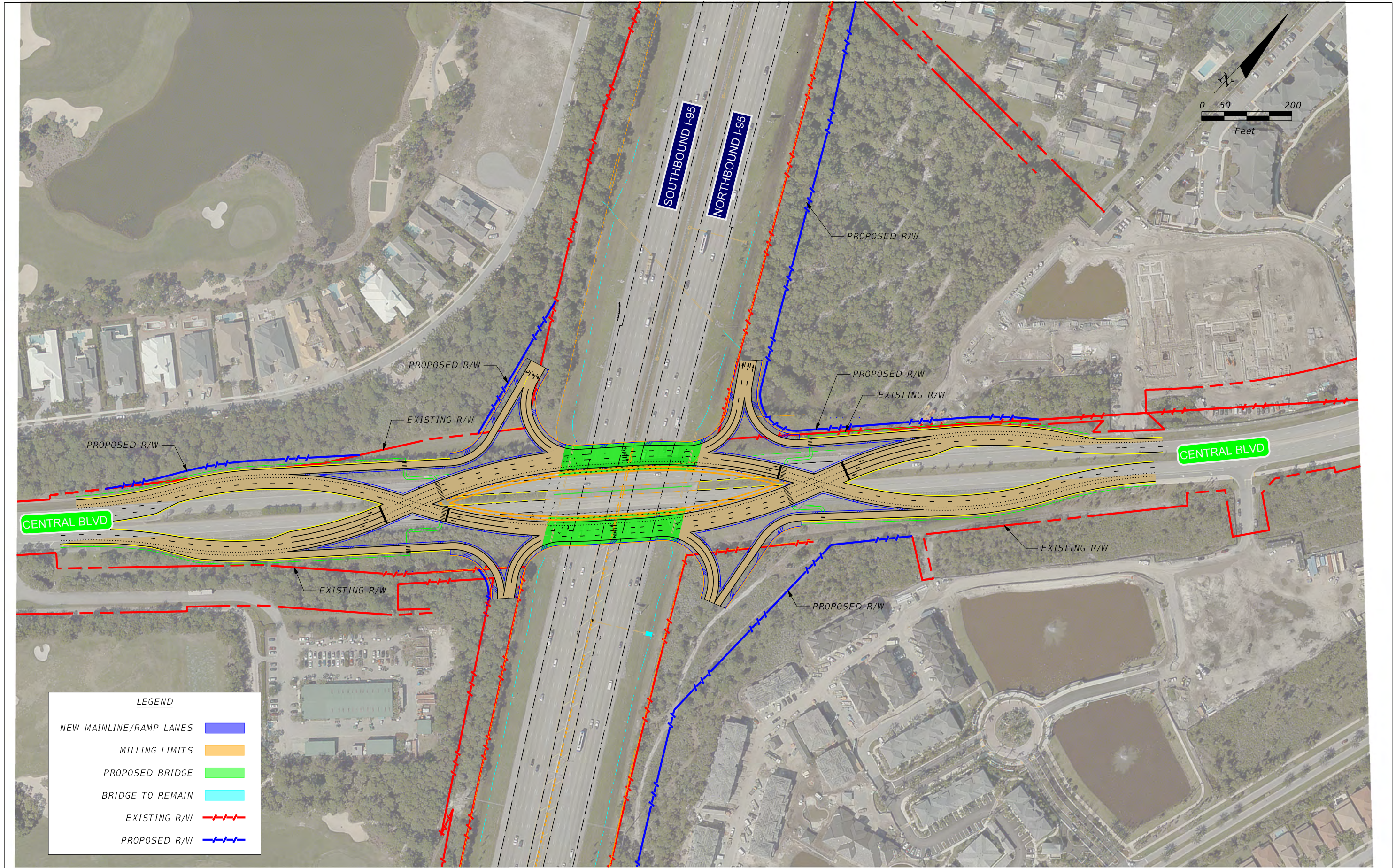
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APPENDIX B

Summary of Public Comments

I-95 at Central Boulevard Interchange PD Public Comment Summary

No.	Title	First Name	Last Name	Company	Neighborhood	Representing	Address	City	State	Zip	email	Phone	Comment Receipt Date	Comment Delivered How?	Comment
1	Mr.	Edward	Salvatore		Hampton Cay		3144 Yorkshire Lane	Palm Beach Gardens	FL	33418	Esalva6347@aol.com	917-796-2435	2/18/2016	APW	Any addition of traffic on Central will cause a huge problem. There is a school close to the sight. People jogging, walking, children, traffic to enter the school. There is always traffic. Please reconsider this sight. Thank you
2	Mrs.	Aminda	Salvatore		Hampton Cay		3144 Yorkshire Lane	Palm Beach Gardens	FL	33418		561-318-5735	2/18/2016	APW	Definitely opposed to interchange location PGA & Central. Heavy traffic already due to school and more homes are being built. Homeowners from our community will move.
3	Ms.	Tara	Davis	Johnston Group Development			5090 PGA Boulevard, Suite 212	Palm Beach Gardens	FL	33418	tara@johnstongroupinc.com	561-262-6714	2/18/2016	APW	Diamond design as opposed to the braid - The braid seems to be confusing & more likely to be accident prone.
4	Mr.	Ryan	Johnston	Johnston Group Development			5090 PGA Boulevard, Suite 212	Palm Beach Gardens	FL	33418	ryan@johnstongroupinc.com	561-691-4552	2/18/2016	APW	I think the best interchange plan is the tight diamond. There seems to be more confusion associated with the braided intersection which could be a safety issue. Bringing traffic (I-95 north exit) to a complete stop in a traditional configuration seems to be a preferred approach.
5	Ms.	Kelly	Cheary		Bent Tree		443 Woodview Cir.	Palm Beach Gardens	FL	33418	kcheary@att.net	561-694-8875	2/18/2016	APW	I think this is a bad idea. It will cause major traffic and noise in a predominant residential neighborhood area and increased safety concerns for the schools in the area. There has got to be some other option. You are only talking about peak traffic time issues - this is too much disruption to our tranquility and too much taxpayer expense for such small periods of time.
6	Mr.	Kevin	Shapiro	Shapiro/Pertnoy			3222-C Commerce Place	West Palm Beach	FL	33407	kevin@shapiropertnoy.com	561-793-5852	2/18/2016	APW	I believe the tight diamond is a great design. It accomplishes many things related to traffic. Hopefully this can be fast tracked as there is a great need. Travel times will be greatly reduced with this plan. Thank you.
7	Mr.	Michael	Bloom		Hampton Cay		2016 Chelsea Pl.	Palm Beach Gardens	FL	33418	mustangmb67@aol.com		2/18/2016	APW	This is a residential area that will be driven to be a Military Trail situation. The FDOT should be fixing the existing PGA on and off ramps. How these were ever approved?? Why ... This be drives to Donald Ross where Alton ... Is going in? To sum it up money would be better spent fixing existing infrastructure that are poorly designed.
8	Mr.	Steven	Shapiro	Shapiro/Pertnoy			3222-C Commerce Place	West Palm Beach	FL	33407	steven@shapiropertnoy.com	561-793-5852	2/18/2016	APW	After reviewing the various versions & the displays, it seems the interchange will do much for traffic on and around PGA Blvd. The Braid plan seems dangerous, having experienced other similar designs where auto lights face auto lights at night. Its scary & has had its share of accidents. We prefer the tight diamond. Its simple, functional & achieves the same result.
9	Mr. & Mrs.	Ben and Stacey	Fowler		Hampton Cay		4023 Kingston Lane	Palm Beach Gardens	FL	33418	Fowler33418@gmail.com	561-306-5862	2/18/2016	APW	We are opposed to any interchange at Central Blvd. The problem with traffic at the 95 PGA Blvd is due to the narrowing of the interchange for traffic getting on 95 south of PGA. All west bound traffic on PGA is routinely backed up to Prosperity Farms as a direct result of the 1 lane on ramp at PGA. Therefore, I would support a reconstruction of the PGA Blvd 95 South on ramp. Furthermore, the interchange at Central Blvd is a bad idea as it is in the middle of residential housing and 2 schools. This added traffic on Central Blvd. would cause a more dangerous road adversely affecting the residents and students at the schools. Therefore, I am opposed to the proposed interchange at Central Blvd.
10	Ms.	Eileen	Anderson							33418	Anderse@mail.com		2/18/2016	APW	Safety was not mentioned in any of the bulletins posted. Will there be lanes for cycling and walkways for walking?
11	Mr. & Mrs.	Ron and Nancy	Atchley		Shady Lakes		5015 Whispering Hollow	Palm Beach Gardens	FL	33418	nancyandron@att.net		2/18/2016	APW	We don't see how this interchange will significantly help the problem of traffic on PGA Blvd. People using PGA Blvd. are going to the Mall or other businesses or to the west to the Turnpike. We don't see them using Central Blvd. interchange.

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No.	Title	First Name	Last Name	Company	Neighborhood	Representing	Address	City	State	Zip	email	Phone	Comment Receipt Date	Comment Delivered How?	Comment
12	Mr.	Ken	Tuma	Urban Design Kiday Studios			610 Clematis St, #2	West Palm Beach	FL	33401		561-366-1100	2/18/2016	APW	Representing impacted property owner on Central Gardens.
13	Ms.	Rita	Sonberg		Bent Tree		113 Bent Tree Dr.	Palm Beach Gardens	FL	33418	rsonberg3@hotmail.com	561-691-9277	2/18/2016	APW	We hope you can delay this I95/Central Blvd interchange at least 20 years (twenty)?? Today the traffic on Central Blvd approaching PGA Blvd was backed up past our community entrance. I can only imagine how much worse it would be with an extra amount of traffic from the new ramps that are proposed, I'm also concerned about safety of children & all residents. I beg you to not put this project into effect! Thanks for reading my concerns.
14	Mr.	Octavio 'Oats'	Reis, PE	Bowman Consulting		Old Palm CDD	301 SE Ocean Blvd., Suite 301	Stuart	FL	34994	oreis@bowmancg.com	772-283-1413	2/18/2016	APW	The Old Palm Community Development District owns property on the NW and SW corners of I-95 & Central Blvd. The areas abutting I-95 within our property are dedicated as preserve areas. FDOT should not consider any interchange alignments that will require additional right of way from our property, thus impacting the CDD preserve areas.
15	Ms.	Sharon	Long		Garden Woods		4267 Linden Avenue	Palm Beach Gardens	FL	33410	wmarussich@comcast.net	561-626-0406	2/18/2016	APW	I am in favor of the I-95 interchange at Central Blvd. I would prefer alternatives 2 for safety reasons. However if it cannot be built due to lack of available funds I would settle for alternatives 3. An interchange is essential looking at the projected traffic for 2040 shown at this meeting. Thank you for holding this workshop.
16	Mr.	Darryl	Miller		San Michelle			Palm Beach Gardens	FL	33418	mbchb1994@yahoo.com		2/18/2016	APW	I oppose the plan because it is distributing the problem throughout the city as opposed to keeping it in pockets. If you do proceed: consider putting a Turnpike on ramp on Donald Ross to help redirect existing Central blvd traffic that includes SWA trucks etc & thereby decrease some traffic prior to offsetting it with new I95 traffic on central.
17	Mr.	Al	Gyuricza		Hampton Cay		3140 Yorkshire Lane	Palm Beach Gardens	FL	33418	agyuricza666@gmail.com	561-557-4933	2/18/2016	APW	PLEASE DO NOT BUILD THESE RAMPS: 1. Presently we cannot get out of our community with all the traffic on Central Blvd (we are at the intersection of PGA Blvd & Central). Traffic is already backed up. 2. With both lanes on Central blocked emergency vehicles will not be able to get through the blocked lanes. 3. It is a known fact that residential communities near highway interchanges are more likely to be burglarized. Criminals from the Miami area & Port St Lucie will burglarize homes, hop onto 95 and speed away. 4. Children & adults use the sidewalks. They walk & ride their bikes to school. Adults joy ride their bikes, and walk on the sidewalks. Now an interchange will block their route & make it dangerous to get across the entrance & exit ramps. 5. The ramps will be unsightly & overwhelming. They do not belong in a residential area or community. Values of our homes will be greatly affected. 6. Children/ buses/ cars will be late for school with all the additional traffic. As it is, students are late getting to school & leaving especially with the present traffic. What will happen with additional traffic. 7. Presently, it is impossible to get to our tennis courts because of the Central Blvd & school traffic backups. What will happen with additional traffic. 8. People will use PGA Blvd & Central Blvd as a short cut between the Florida Turnpike & Rt 95 Traffic will be horrific. 9. The idling cars and traffic noise will greatly affect PBG community & residential areas. 10. Exit & entrance ramps to 95 do not belong in residential communities. All other exits are in commercial areas.
18	Mr.	Benjamin	Fowler								befowler@mail.smu.edu		2/3/2016	email	Thank you. I want to be informed of the progress of this study. I live on Central Blvd. and it is my belief that the root cause of the majority of the traffic problems in Palm Beach Gardens is a result of the insufficient interchange a PGA and I-95. At this interchange 2 lanes merge to 1 on the southbound ramp of 95 which cause backups on PGA Blvd heading west all the way back to Prosperity Farms Rd. Creating a new interchange on Central Blvd in the middle of residential housing will not fix the root cause of these traffic problems. Therefore, I am opposed to the proposed project at Central but would support the reconstruction of the interchange at PGA and I-95 to eliminate the merge from 2 lanes to 1 on the southbound ramp. Thank you, Ben Fowler.

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No.	Title	First Name	Last Name	Company	Neighborhood	Representing	Address	City	State	Zip	email	Phone	Comment Receipt Date	Comment Delivered How?	Comment
19	Ms.	Linda	Mancino		Westwood Lakes						lmancino@dpclinical.com	301-461-4220	2/3/2016	email	I attended the kick off meeting that was held in PBG 1 year ago. Regrettfully, I will be out of town for the next meeting on 2/18/16. I have so much riding on this decision and want to implore DOT to NOT put this interchange on the proposed Central Blvd site. My home sits at the very top of the blue circle in the picture below. So close it's almost touching my street. I would think with something this important and life changing for some people you would have used current maps. This map makes it appear that there is wooded land in abundance surrounding this proposed site. This is absolutely false. There is a large community under construction on the East side and on the West side of Central Blvd an Assisted Living facility is also under construction. It hurts down deep to see trees and quiet being replaced by concrete and parking lots but I get the whole progress thing; however, Central Blvd should remain as it is as it would impact the residents too adversely. It's too close to residential. Plus Central Blvd has a road to a school that is too close. The areas of PGA and Military Trail should be strongly considered as I feel it has less impact on the people that live in this area 24/7. I'm not stupid, it all comes down to \$\$\$\$\$. With an interchange at Central it would have such a negative impact on our property values in addition to our quality of life with the noise, dirt and appearance. I had plans to re-model my kitchen but now I'm hesitant because the of the possibility that my home value will take a hit. Please do not wreck Central Blvd. with this interchange.
20	Ms.	Kristin	Shapiro				12411 Aviles Circle	Palm Beach Gardens	FL	33418		561-866-9615	2/21/2016	email	Bing Wang, This email is in follow-up to Thursday's open house regarding the I-95 study (PGA Blvd/ Central Blvd). They suggested to email you our comments. This project is not taking into effect a variety of problems: 1. The main congestion is on PGA Blvd. Redirecting traffic to the Central Blvd area is not going to fix the entrance and exit ramps problems at PGA Blvd. PGA exits need to be studied and re-engineered for better traffic flow before another on and off ramp is created down the road. 2. If the Central Blvd ramp is created, it will be the only i-95 exit in Palm Beach county to dump traffic into a residential community area. There isn't any commercial business on Central, therefore the commercial business traffic will still be an issue on PGA. 3. There are 5 schools within one mile on Central Blvd; increasing traffic in this area will not help keep our school children who walk and ride bikes safe nor will it help our high school new drivers be any safer (especially with the interchanges that are proposed!) Thank you for giving us residents a chance to voice our concern and visit the open house to see the proposals. Unfortunately, I do not agree that creating this additional ramp at Central will help the main problem of PGA Blvd commercial traffic. I would rather see FDOT spend money to fix those on and off ramps and make them more efficient than spend money on an additional ramp that wont help the original problem. Thank you for giving us residents a chance to voice our concern and visit the open house to see the proposals. Unfortunately, I do not agree that creating this additional ramp at Central will help the main problem of PGA Blvd commercial traffic. I would rather see FDOT spend money to fix those on and off ramps and make them more efficient than spend money on an additional ramp that wont help the original problem. Sincerely, Kristin Shapiro
21	Mr.	David	Brasher	Triton Atlantic Partners		Old Palm Holdings	11089 Old Palm Dr.	Palm Beach Gardens	FL	33418	dbrasher@tritonatlantic.com	561-209-8105	3/7/2016	email	Bing, I have been asked by our sales department If the State was considering a sound barrier wall at our project location. Please get back to me when you have a chance. Thanks, Dave

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No.	Title	First Name	Last Name	Company	Neighborhood	Representing	Address	City	State	Zip	email	Phone	Comment Receipt Date	Comment Delivered How?	Comment
22	Mr.	Joe	Roche								jaksroche@aol.com		2/2/2016	email	Can you please add me to any email updates for this project Thank you Joe Roche
23	Ms.	Bella	Perez								reception@fullcommp.com		2/12/2016	email	Good afternoon Ms. Wang, Please provide answers regarding FDOT Project: I-95/PGA Boulevard/Central Boulevard at your earliest convenience. Thank you in advance. 1. Are you still the Project Manager? 2. What is the status of the project? 3. Is there funding available for R/W and/or construction? a. If so, please provide the budgets allocated for Right of Way and for Construction phases b. If not, do you know when these will be approved? 4. When are appraisals scheduled to commence? 5. Please provide me with the most current Maps, Plans, Sketches, Alternatives, Pond Siting Reports or anything in that nature 6. When are RW offers/acquisitions and construction scheduled to commence? 7. What is the project number? I reviewed upcoming Public Hearings and found no project number.
24	Mr.	Andrew	Weidman	Seminole Tribe of Florida							andrewweidman@semtribe.com	863-983-6549,,12216	2/10/2016	email	Dear Mrs. Wang, The Seminole Tribe of Florida’s Tribal Historic Preservation Office (STOF-THPO) thanks you for contacting the Tribe regarding the I-95 at PGA Boulevard/Central Boulevard PD&E Study Workshop in Palm Beach County. The STOF-THPO has received the invitation to the public hearing and we are unfortunately unable to attend. However, because the project lies within an area that is of historical importance to the Tribe, we would like to ensure that adequate provisions are made to identify and assess any unidentified historic properties that may be present within the final corridor. Please notify the STOF-THPO when a final corridor has been selected for evaluation. Additionally, we respectfully request to be notified by FDOT of any developments regarding this project. Respectfully, Andrew J. Weidman, RPA Compliance Review Specialist Tribal Historic Preservation Office Seminole Tribe of Florida Phone: (863) 983-6549 ext. 12216 Email: andrewweidman@semtribe.com

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No.	Title	First Name	Last Name	Company	Neighborhood	Representing	Address	City	State	Zip	email	Phone	Comment Receipt Date	Comment Delivered How?	Comment
25	Mr.	Steven	Shapiro	Shapiro/ Pertnoy			3222-C Commerce Place	West Palm Beach	FL	33407	steven@shapiro rtnoy.com	561- 793- 5852	2/22/2016	email	Good Morning, Bing. It was nice to see you again at the meeting last week. I have a technical question about the southbound lane on Alternative 2. Do the I-95 southbound cars that get off on Central Blvd have the ability to make a left turn to go NE on Central. Is there a light planned for that off ramp? Thanks. Steven M. Shapiro Shapiro Pertnoy Companies 3222-C Commerce Place West Palm Beach, FL 33407 561.793.5852 o 561.346.3478 c 561.688.5868 f steven@shapiropertnoy.com www.shapiropertnoy.com
26	Mr.	Martin	Trent		Paloma		4839 Cadiz Cir	Palm Beach Gardens	FL	33418	martin4839@com cast.net		2/9/2016	email	Ms. Wang: Please add my email address for additional notifications concerning this project. martin4839@comcast.net For the record: I am Opposed to Alternatives 2 and 3 as they will result in greatly increased traffic on Victoria Falls Blvd through the heart of the residential Paloma community for use as a short cut from Central Blvd and Military Trail to and from the proposed new interchange. The No Build Alternative is the only alternative which will not negatively impact the quality of life for the Paloma residential community. If an interchange is chosen over the needs of Paloma residents, mitigation measures are required to prevent the intrusion of the additional traffic resulting on Victoria Falls Blvd. The measures include the complete elimination of all lanes of Victoria Falls Blvd west from Elm Ave to Central Blvd and the complete elimination of all lanes of Elm Ave north from Victoria Falls Blvd to Dulce Ct. Sincerely, Martin Trent Palm Beach Gardens, FL 33418
27	Mr.	Mark	Troen				7217 Oxford Court	Palm Beach Gardens	FL	33418	Mtroen@winnmar k.com	561- 855- 4415	2/1/2016	email	Dear Ms. Wang, I live in the affected area of these proposed improvements. Why is Hood Road not being considered for an interchange? It is further from Donald Ross than Central is from the PGA interchange on I-95. Thus there is ample room for interweaving exit/entrance ramps. Furthermore, new interchange at the Hood Road location would service all of Palm Beach Gardens and provide relief to PGA/I-95. An interchange at Central may provide relief for the PGA interchange but would unnecessarily burden Central – which at the current time and in the future will have NO additional commercial or additional development. As such, this proposed project area appears far too limited to provide the proper solutions and instead creates an overloaded use of Central instead. I would appreciate a direct response to these concerns as I am unable at this time to attend the public workshop on February 18th Regards, Mark Troen 7217 Oxford Court Palm Beach Gardens, FL 33418 Mtroen@winnmark.com (561) 855-4415
28	Ms.	Darlene	Collins								dee2collins@msn. com		4/5/2016	email	As a resident who lives off of Central Blvd and Hood Road, I am strongly opposed to this project. This is a residential area with 5 schools. This is NOT the solution. Sincerely, Darlene Collins

I-95 at Central Boulevard Interchange PD Public Comment Summary

No.	Title	First Name	Last Name	Company	Neighborhood	Representing	Address	City	State	Zip	email	Phone	Comment Receipt Date	Comment Delivered How?	Comment
29	Ms.	Amber	Skelly	Gray Robinson			301 S. Bronough Street, Suite 600	Tallahassee	FL	32301	Amber.Skelly@gray-robinson.com	850-577-9090	5/9/2016	email	Good morning, Can you add me to the list to receive email updates for the I-95 @ PGA Boulevard interchange project in Palm Beach County? Thanks, Amber
30	Mr.	Darrel	Donatto				527 Commons Drive	Palm Beach Gardens	FL	33418	ddonatto@gmail.com	561-719-3517	9/8/2016	email	Thanks so much for the letter regarding this project and the opportunity to comment as a stakeholder nearby the project. I would urge the DOT to go to Central Blvd, and travel southbound from Hood Rd to PGA Blvd during the morning school arrival hours. The traffic is generally backed up from 117th Ct North to over the I-95 overpass bridge. This construction project should not only address the need for access to Central - but should include provisions for dealing with the current traffic load that is beyond the current capacity of the road. Should people attempt to exit from I-95 on to Central when the traffic is backed up for school arrivals, they may get backed up on to I-95 and cause a traffic hazard that is dangerous to I-95 travelers. Please include my comments into the record. Respectfully
31	Ms.	Kimberly	Warth LaBarbiera		Old Palm		12218 Tillinghast Circle	Palm Beach Gardens	FL	33418	warthka@gmail.com	201-961-5104	9/11/2016	email	Hi I am a resident in Old Palm Development. I want to know what does the "potential interchange" mean. Adding a ramp off 95 to central blvd? Expanding which roads? You should provide more information to allow the residents within 300 feet to truly think about and access this information prior to the meeting so the meeting can be more productive and have greater dialogue. Please be specific. I want the information prior to the meeting. Residents in Old Palm love the fact that Central Blvd, is not highly traveled and does not have a lot of traffic. This is considered private and safe away from intruders not having a highway ramp, or expanded roadways with a lot of traffic.
32	Ms.	Rebecca	Mulchay	Pinder Troutman			2005 Vista Parkway, Suite 111	West Palm Beach	FL	33411	rmulcahy@pindertroutman.com	561-296-9698	9/12/2016	email	Hello, I would like to get a copy of the subject project's IJR. Can you send it to me, or send me a link to it? Thank you.
33	Ms.	Kimberly	Warth LaBarbiera	Morgan Ryan Design							warthka@gmail.com	201-961-5104	9/12/2016	email	They are not specific. What are they trying to accomplish? More lanes on central ?? Hood?? An exit or entrance onto hood from 95?
34	Ms.	Kimberly	Warth LaBarbiera	Morgan Ryan Design							warthka@gmail.com	201-961-5104	9/14/2016	email	So help me understand. On this 1.46 acre parcel you guys are looking to build a road/ramp for I-95 access??

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No.	Title	First Name	Last Name	Company	Neighborhood	Representing	Address	City	State	Zip	email	Phone	Comment Receipt Date	Comment Delivered How?	Comment
35	Ms.	Maria	Shanahan		PGA Commons							716-491-5406	9/19/2016	email	Please Include me on your email updates for the 95 PGA Central Blvd project. Best regards,
36	Mr.	Martin	Trent		Paloma		4839 Cadiz Cir	Palm Beach Gardens	FL	33418			9/13/2016	mail	Dear Mr. Wang: I oppose the construction of an I-95 interchange at this Central Blvd because it will degrade our safety, solitude, tranquility and quality of life in the Paloma neighborhood. The interchange will cause a large increase of traffic and heavy trucks to utilize Victoria Falls Blvd as a shortcut from Central Blvd to Military Trail. None of the alternaitves discussed propose mitigation measures or prevention of this increase in traffic through a quiet residential neighborhood. If FDOT approves this interchange project, I will seek to have the Paloma HOA (397 homes) file a lawsuit to stop the project. I trust that you will place the wishes of our residential community above the small inconvenience of no exit at Central Blvd to the traveling public. Sincerely, Martin Trent, 4839 Cadiz Circle, Palm Beach Gardens, FL 33418
37	Mr.	Todd	Engle	City of Palm Beach Gardens			10500 N Military Trail	Palm Beach Gardens	FL	33408	tengle@pbgfl.com	561-804-7012	9/28/2016	PH - written	The Garden Lakes Community is adjacent to the I-95 south bound ramp onto Military Trail. Several residents has requested a buffer (landscape, wall, etc) for their community. The proposed condition will effect homes off of 4th Lane, 6th Lane 8th Lane and 10th Lane. The buffer could be a landscape berm, wall or any combination there of.
38	Dr.	Robert A	Strasser				118 Satinwood Lane	Palm Beach Gardens	FL	33410	rstrasser@aol.com	561-373-3853	9/28/2016	PH	Absurd- Major traffic problem is east to west. This is another pathetic waste of \$\$ to preserve beuracracic jobs. Great for the trial attorneys. Bing could not answer most questions.
39	Mr.	Richard	Alman		Hampton Cay		4045 Kingston Lane	Palm Beach Gardens	FL	33418	richard.c.alman@gmail.com	972-679-6894	9/28/2016	PH - written	Ms. Wang - This proposed interchange willl be the first constructed in a residential neighborhood in the United States. This project should not gor forward. But, if it does, and if indeed your plan will consider the safety of the children going to and from school on 117th Court, the bicycle traffic, and pedestrian residents, you must incorporate an unobstructed elevated walkway/bike way, without signal crossings in your plan and budget. Again, this project should not go forward.
40	Ms.	Betsy	Strasser		Sanctuary		118 Satinwood Lane	Palm Beach Gardens	FL	33410			9/28/2016	PH - written	I feel this is a done deal & this is a dog & pony show. Thee is no county/town planning. You are letting developers run the show. This would not be needed if uncontrolled growth were not allowed. 15 years ago when I moved here there was not a shooting a day in PB County. I did not try to change it to where I came from. All this growth has done nothing for us in the true economics. I 95 was the worst thing that happened to this area. When children walking home from school are injured or killed because fo the # of nearby schools, Do not blame me.
41	Ms.	Kathy	Gilbert	City of Palm Beach Gardens			10500 N Military Trail	Palm Beach Gardens	FL	33458	kgilbert@pbgfl.com	561-799-4292	9/28/2016	PH - written	Please consider adding a sound wall and/or landscape buffer next to offramp on SB Military Trail adjacent to the residentialneighborhood of Garden Lakes.
42	Mr.	Travis	Douglas	Bowman Consulting Group			301 SE Ocean Blvd. Suite 301	Stuart	FL	34994	TDouglas@BowmanCD.com	772-283-1413	9/28/2016	PH - written	Old Palm CDD owns property on the NW and SW corners of the proposed I-95/Central Blvd Interchange. These properties are required to be preserved in their natural vegetated state by the City of Palm Beach Gardens as part of the site plan approval for the Old Palm Subdivision. Per previous comments issued during the preliminary public meetings regarding this PDE study, we continue to object for any alignment alternatives that impactour preserve areas. Thank you

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No.	Title	First Name	Last Name	Company	Neighborhood	Representing	Address	City	State	Zip	email	Phone	Comment Receipt Date	Comment Delivered How?	Comment
43	Mr.	Sam	Wiley		Bent Tree		461 Woodview Circle	Palm Beach Gardens	FL	33418	swiley@czi-inc.com	561-747-7455	9/29/2016	written form by email	Please maintain safe/unimpeded pedestrian and bicycle lanes along Central Blvd through the interchange. Please address gopher tortoise mitigation of impacts adjacent to South Gardens.
44	Ms.	Vicki	Copani		Legends at the Garden		4931 Bonsai Cir., #111	Palm Beach Gardens	FL	33418	luxuryrealtor@vickicopani.com	561-301-1463	10/1/2016	email	I was in attendance at the Public Hearing, Wed. Sept. 28, 2016 at the City of Palm Beach Gardens, 10500 North Military Trail, Palm Beach Gardens, FL 33410. My Name: Vicki Copani, Company/Neighborhood: Live at Legends at the Gardens, 4931 Bonsai Cir., #111, Palm Beach Gardens, FL 33418 and work at: Lang Realty/PBG, 6271 PGA Blvd., #200, Palm Beach Gardens, FL 33418 My Cell Phone: 561-301-1463 Email: LuxuryRealtor@VickiCopani.com My comments are as follows: I both live and work within a mile of the proposed SR R/I-95 at Central Blvd. Interchange. I pass through this intersection several times each day, both for business and personal use. This is a highly residential neighborhood. There are six schools within a 1 mile radius; and I'll state for the record that they are Timber Trace Elementary, Watson P. Duncan Middle School, Dwight D. Eisenhower High School, Benjamin High School, Marsh Pointe Elementary, and the newest addition on Hood Rd. is Franklin Academy. Within the 1-2 mile radius are all residential communities, Old Palm, Paloma, Hamptons, Quay, Hamptons Townhomes, Evergrene, The Isles, Magnolia, Cielo, Fiore and my community of Legends at the Gardens. A little over that radius is Westwood Gardens, Trevi at the Gardens, Eastpointe Country Club, Old Marsh and Mirabella. With the exception of the new Frenchman's Reserve Publix shopping center on the corner of Alternate A1A and Hood, there are no other commercial properties that have been developed. Kolter's newest project, Altonlife, is a little farther West on Hood Road, past Franklin Academy. This community can also be accessed immediately at the intersection Central Blvd. and Grandaflorea and of I-95 and Donald Ross Road. It seems ridiculous to spend several million dollars for this interchange in this location, in the middle of a 100% residential community with 6 schools. The driving distances from the PGA Exit, Military Exit and Donald Ross Exits off I-95 are literally and physically less than 5 minutes apart, even during in heavy traffic times. This roadway, Central Blvd. from PGA Blvd. to Donald Ross and Hood Road to and from Jog Rd.; is the most visually pleasant ride in the entire area. The landscaping and sidewalks make this a very enjoyable drive. Why would you obstruct this roadway with several years of needless construction and several million dollars of unnecessary taxpayers monies? What will it take to abort this project? Respectfully submitted,
45	Mr.		Landi										9/27/2016	phone	
46	Ms.	Maria	Marino			Palm Beach Gardens City Council	906 Windemere Way	Palm Beach Gardens	FL	33418			9/28/2016	PH speaker	See Public Hearing Transcript
47	Mr.	Don	Mathis				146 Thornton Drive	Palm Beach Gardens	FL	33418			9/28/2016	PH speaker	See Public Hearing Transcript
48	Ms.	Betsy	Strasser				Satinwood Lane						9/28/2016	PH speaker	See Public Hearing Transcript
49	Ms.	Linda	Mencino				12724 Woodmill Dr	Palm Beach Gardens	FL				9/28/2016	PH speaker	See Public Hearing Transcript

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No.	Title	First Name	Last Name	Company	Neighbor- hood	Repre- senting	Address	City	State	Zip	email	Phone	Comment Receipt Date	Comment Delivered How?	Comment
50	Mr.	Richard	Alman		Hampton Cay		4045 Kingston Lane	Palm Beach Gardens	FL	33418			9/28/2016	PH speaker	See Public Hearing Transcript
51	Mr.	Kevin	Foley				12056 SE Birkdale Run	Tequesta	FL	33469			9/28/2016	PH speaker	See Public Hearing Transcript
52	Ms.	Marcie	Tinsley			Palm Beach Gardens City	10500 N Military Trail	Palm Beach Gardens	FL	33410			9/28/2016	PH speaker	See Public Hearing Transcript
53	Ms.	Kimberly	La Barbiera										9/28/2016	PH dictated	See Public Hearing Transcript
54	Ms.	Cheryl	Casagrande										9/28/2016	PH dictated	See Public Hearing Transcript



APPENDIX C

ETDM Summary Report

ETDM Summary Report

Project #13748 - Interchange Improvements to SR 9 (I-95) at PGA Boulevard and Central Boulevard

Final Programming Screen - Published on 07/03/2013

Generated by Shandra Davis-Sanders (on behalf of FDOT District 4)

Printed on: 7/03/2013

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Introduction to Programming Screen Summary Report

The Programming Screen Summary Report shown below is a read-only version of information contained in the Programming Screen Summary Report generated by the ETDM Coordinator for the selected project after completion of the ETAT Programming Screen review. The purpose of the Programming Screen Summary Report is to summarize the results of the ETAT Programming Screen review of the project; provide details concerning agency comments about potential effects to natural, cultural, and community resources; and provide additional documentation of activities related to the Programming Phase for the project. Available information for a Programming Screen Summary Report includes:

- Screening Summary Report chart
- Project Description information (including a summary description of the project, a summary of public comments on the project, and community-desired features identified during public involvement activities)
- Purpose and Need information (including the Purpose and Need Statement and the results of agency reviews of the project Purpose and Need)
- Alternative-specific information, consisting of descriptions of each alternative and associated road segments; an overview of ETAT Programming Screen reviews for each alternative; and agency comments concerning potential effects and degree of effect, by issue, to natural, cultural, and community resources.
- Project Scope information, consisting of general project commitments resulting from the ETAT Programming Screen review, permits, and technical studies required (if any)
- Class of Action determined for the project
- Dispute Resolution Activity Log (if any)

The legend for the Degree of Effect chart is provided in an appendix to the report.

For complete documentation of the project record, also see the GIS Analysis Results Report published on the same date as the Programming Screen Summary Report.

#13748 Interchange Improvements to SR 9 (I-95) at PGA Boulevard and Central Boulevard

District: District 4

County: Palm Beach

Planning Organization: FDOT District 4

Plan ID: Not Available

Federal Involvement: Federal Action Federal Funding

Phase: Programming Screen

From:

To:

Financial Management No.: 41326512201

Contact Information: Patrick Raymond Glass (954) 777-4681 patrick.glass@dot.state.fl.us

Snapshot Data From: Programming Screen Summary Report Re-published on 07/03/2013 by Shandra Davis-Sanders

Issues and Categories are reflective of what was in place at the time of the screening event.

	Natural											Cultural			Community						
	Air Quality	Coastal and Marine	Contaminated Sites	Farmlands	Floodplains	Infrastructure	Navigation	Special Designations	Water Quality and Quantity	Wetlands	Wildlife and Habitat	Historic and Archaeological Sites	Recreation Areas	Section 4(f) Potential	Aesthetics	Economic	Land Use	Mobility	Relocation	Social	Secondary and Cumulative Effects
Alternative #1 From: South of PGA Blvd To: North of Central Blvd Re-Published: 07/03/2013 Reviewed from 10/05/2012 to 11/19/2012)	0	0	3	2	0	0	0	2	3	3	3	3	3	3	2	1	2	1	3	3	2

During Project Development, any required Section 4(f) Determination of Applicability (DOA) documents will be coordinated with FHWA and will focus on the avoidance and/or minimization of impacts to the cited resources. Required DOAs for cultural resources will be assessed after a Cultural Resource Assessment Survey is conducted and NRHPeligibility is determined. Once NRHPeligibility is determined, any project effects will be investigated/coordinated with FDOS and the need for any Section 4(f) documents will be assessed.

Degree of Effect: 3 *Moderate* assigned 11/19/2012 by Linda Anderson, Federal Highway Administration

Coordination Document: PD&E Support Document As Per PD&E Manual

Direct Effects

Identified Resources and Level of Importance:

Within 100' buffer:

1. PB13330 - Old Dixie Highway.
2. PB 13795 - Military Trail.
3. PB12102 - Florida East Coast Railway.
4. 287 acres of Greenways Ecological Priority Linkages.

Within 200' buffer:

387 acres of Greenways Ecological Priority Linkages.

Within 500' buffer:

693 acres of Greenways Ecological Priority Linkages.

Within 1,320' buffer:

1. 1,536 acres of Greenways Ecological Priority Linkages.
2. Gardens Park.
3. Gardens Tennis Center.

Adjacent to I-95/Central Blvd. proposed interchange:

Golf course in NW and SW quadrants.

What appears to be a greenway laced with trails in SE quadrant.

Comments on Effects to Resources:

Two of the threeSection 106 resources described above have not been evaluated for NRHP-eligibility; the third resource is eligible. Impacts to NRHP-eligible properties may be Section 4(f) resources.

Regarding the numerous acres of Greenways Ecological Priority Linkages, significant, yet unbuilt, publicly-owned, and publicly-accessible parks, trails, recreation areas, and wildlife refuges designated as such in a city or county master plan are considered Section 4(f) resources.

If the golf course and the trail-laced greenway adjacent to the proposed interchange at I-95/Central Blvd. are publicly owned and publicly accessible, they may be Section 4(f) resources.

A Section 4(f) DOA may be required.

Additional Comments (optional):

CLC Commitments and Recommendations:

ETAT Reviews and Coordinator Summary: Community

Aesthetics

Project Effects

Coordinator Summary Degree of Effect: 2 *Minimal* assigned 01/14/2013 by FDOT District 4

Comments:

Noise and vibration related effects may be of concern to proximate residences and businesses since the project traverses a dense urban area. However, given the presence of noise barriers and the fact that the project supports the land use vision depicted through the Comprehensive Plans of the City of Palm Beach Gardens and Palm Beach County, impacts to aesthetics are anticipated to be minor. Therefore, a Summary DOE of Minimal has been assigned to the Aesthetics issue.

Potential aesthetic effects from the proposed improvements will be assessed during Project Development as more detailed information becomes available. Public outreach will be conducted by FDOT District Four in coordination with the Palm Beach Metropolitan Planning Organization and the City of Palm Beach Gardens to solicit opinions and preferences from residents and businesses on potential project effects and general design concepts related to aesthetics. In addition, noise abatement criteria (as defined per Part 2, Chapter 17, Section 17-5.5 of the FDOT PD&E Manual) will be followed as part of the Noise Study to be performed.

Degree of Effect: 2 Minimal assigned 11/19/2012 by Jorge Padron, FDOT District 4

Coordination Document: To Be Determined: Further Coordination Required

Direct Effects

Identified Resources and Level of Importance:

200-Foot Buffer:

2010 Census Designated Places (1)

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]
- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (10)

Geocoded Homeowner and Condominium Associations (1)

- TRAILS END VILLAS

Geocoded Laser Facilities (1)

- DERMA SILKE, LLC

Cultural Field Survey Areas (5)

Resource Groups (3)

FDOT RCI Bridges (10)

Noise Barriers (5)

Facility Crossings (15)

Greenways Ecological Priority Linkages (Low 99.3 Acres)

SFWMD Residential Areas 2008

- 1210 FIXED SINGLE FAMILY UNITS / 4.0 / 1.03%
- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 31.5 / 8.17%

Residential Areas - Future Land Use 2008 / Acres / Percent

- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 34.7 / 8.99%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 59.9 / 15.52%

500-Foot Buffer:

2010 Census Designated Places (1)

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]
- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (11)

Geocoded Homeowner and Condominium Associations (1)

- TRAILS END VILLAS

Geocoded Laser Facilities (2)

- DERMA SILKE, LLC
- LASER & EYE SURGERY CENTER OF PALM BEACH

Cultural Field Survey Areas (6)

Resource Groups (3)

FDOT RCI Bridges (10)

Noise Barriers (5)

Facility Crossings (15)

Greenways Ecological Priority Linkages (Low 185.5 Acres)

SFWMD Residential Areas 2008

- 1210 FIXED SINGLE FAMILY UNITS / 23.6 / 3.41%
- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 78.6 / 11.38%
- 1340 MULTIPLE DWELLING UNITS - HIGH RISE / 2.1 / 0.31%
- 1390 HIGH DENSITY UNDER CONSTRUCTION / 0.4 / 0.06%

Residential Areas - Future Land Use 2008 / Acres / Percent

- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 89.2 / 12.91%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 185.2 / 26.80%

Comments on Effects to Resources:

The EST GIS analysis results reveal the following community features within the immediate vicinity of the project (500-foot buffer) that may be sensitive to potential noise and vibration effects: one Census Designated Place (Palm Beach Gardens), one homeowner and condominium association, two laser facilities, cultural resources, and recreational features. Other notable features within proximity to the project (1,320-foot buffer) that may be sensitive to noise and vibration effects include one health care facility (nursing home), two additional homeowner and condominium associations, three additional laser facilities, and three parks; two

Developments of Regional Impact and several Planned Unit Developments, primarily composing portions of the BRPO, are also concentrated in the project area.

While noise and vibration related effects may be of concern to proximate residences and businesses, impacts to aesthetics as a result of the interchange improvements are anticipated to be minimal given the presence of noise barriers and the fact that the project supports the areas land use vision.

Additional Comments (optional):

CLC Commitments and Recommendations:

The following organization(s) were expected to but did not submit a review of the Aesthetics issue for this alternative: Federal Highway Administration

Economic

Project Effects

Coordinator Summary Degree of Effect: **1** *Enhanced* assigned 01/14/2013 by FDOT District 4

Comments:

While potential temporary impacts to businesses may occur during project construction as a result of alterations to vehicular access, overall economic enhancements are anticipated since the project is expected to support the growing bioscience industry and vision of the City and County, as well as the expanding residential, commercial and industrial uses in the vicinity of the interchange. Therefore, a Summary DOE of Enhanced has been assigned to the Economic issue.

During Project Development, public outreach will be conducted by FDOT District Four in coordination with the Palm Beach Metropolitan Planning Organization and the City of Palm Beach Gardens to solicit input from residents and businesses (located within the vicinity of the interchange) regarding potential economic enhancements/impacts as a result of the project.

Degree of Effect: **1** *Enhanced* assigned 11/19/2012 by Jorge Padron, FDOT District 4

Coordination Document: To Be Determined: Further Coordination Required

Direct Effects

Identified Resources and Level of Importance:

200-Foot Buffer:

2010 Census Designated Places (1)

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]

- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (10)

Geocoded Cultural Centers (1)

- ONESSIMO FINE ART (PALM BEACH GARDENS)

Railways

- RAILROAD: 3,229.66 Linear Feet

2008 SFWMD FL Land Use and Land Cover / Acres / Percent

- 1210 FIXED SINGLE FAMILY UNITS / 4.0 / 1.03%
- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 31.5 / 8.17%
- 1400 COMMERCIAL AND SERVICES / 35.8 / 9.28%
- 1411 SHOPPING CENTERS / 16.6 / 4.30%
- 1490 COMMERCIAL AND SERVICES UNDER CONSTRUCTION / 8.2 / 2.12%
- 1820 GOLF COURSE / 5.7 / 1.48%
- 3210 PALMETTO PRAIRIES / 7.7 / 2.00%
- 4110 PINE FLATWOODS / 100.6 / 26.07%
- 4340 UPLAND MIXED CONIFEROUS - HARDWOOD / 1.3 / 0.33%
- 5120 CHANNELIZED WATERWAYS - CANALS / 1.0 / 0.25%
- 5300 RESERVOIRS / 2.9 / 0.75%
- 8140 ROADS AND HIGHWAYS / 170.6 / 44.22%

Future Land Use 2008 / Acres / Percent

- COMMERCIAL, OFFICE, TOURISM, MARINA / 61.2 / 15.87%
- INDUSTRIAL, EXTRACTIVE, TRANSPORTATION / 27.9 / 7.23%
- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 34.7 / 8.99%
- RECREATION/OPEN SPACE / 0.5 / 0.13%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 59.9 / 15.52%
- UNKNOWN, INFORMATION NOT AVAILABLE / 0.0 / 0.01%

500-Foot Buffer:**2010 Census Designated Places (1)**

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]
- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (11)**Geocoded Cultural Centers (2)**

- ONESSIMO FINE ART (PALM BEACH GARDENS)
- STUDIO E GALLERY (PALM BEACH GARDENS)

Railways

- RAILROAD: 3,843.39 Linear Feet

2008 SFWMD FL Land Use and Land Cover / Acres / Percent

- 1210 FIXED SINGLE FAMILY UNITS / 23.6 / 3.41%
- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 78.6 / 11.38%
- 1340 MULTIPLE DWELLING UNITS - HIGH RISE / 2.1 / 0.31%
- 1390 HIGH DENSITY UNDER CONSTRUCTION / 0.4 / 0.06%
- 1400 COMMERCIAL AND SERVICES / 79.0 / 11.44%
- 1411 SHOPPING CENTERS / 22.7 / 3.29%
- 1490 COMMERCIAL AND SERVICES UNDER CONSTRUCTION / 19.5 / 2.82%
- 1700 INSTITUTIONAL / 1.8 / 0.27%
- 1820 GOLF COURSE / 35.6 / 5.15%
- 1850 PARKS AND ZOOS / 4.1 / 0.60%
- 3210 PALMETTO PRAIRIES / 21.2 / 3.07%
- 4110 PINE FLATWOODS / 194.3 / 28.13%
- 4340 UPLAND MIXED CONIFEROUS - HARDWOOD / 3.8 / 0.55%
- 5120 CHANNELIZED WATERWAYS - CANALS / 2.8 / 0.40%
- 5200 LAKES / 0.2 / 0.03%
- 5300 RESERVOIRS / 13.2 / 1.91%
- 8140 ROADS AND HIGHWAYS / 188.0 / 27.20%

Future Land Use 2008 / Acres / Percent

- COMMERCIAL, OFFICE, TOURISM, MARINA / 109.1 / 15.79%
- INDUSTRIAL, EXTRACTIVE, TRANSPORTATION / 60.5 / 8.76%
- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 89.2 / 12.91%
- PUBLIC/SEMI-PUBLIC, GOV, INSTITUTIONAL / 2.4 / 0.35%
- RECREATION/OPEN SPACE / 4.9 / 0.71%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 185.2 / 26.80%
- UNKNOWN, INFORMATION NOT AVAILABLE / 0.1 / 0.01%

Comments on Effects to Resources:

The area surrounding the existing I-95 and PGA Boulevard interchange is urbanized containing a mixture of commercial, industrial, mixed-use and residential land uses with vacant land in the northeast quadrant. According to the City of Palm Beach Gardens Comprehensive Plan, future land use is to remain relatively unchanged, with the exception of the area east of the interchange which has been designated as part of the *Bioscience Research Protection Overlay* (BRPO). The BRPO was developed to protect portions of land for biotechnology/biosciences land uses and includes the Scripps Florida Phase II/Briger Tract DRI which consists of 82 acres located south of Donald Ross Road, north of Hood Road and east and west of I-95 (just north of the study area). The DRI includes 1,600,000 square feet of Biotech Research and Development, 2,400,000 square feet of biotechnological/biomedical, pharmaceutical, and office space, 2,700 residential dwelling units, and 500,000 square feet of retail space. One additional Development of Regional Impact and several Planned Unit Developments, primarily composing portions of the BRPO, are also concentrated in the project area. The project is expected to support the growing bioscience industry and vision of the City and County, as well as the expanding residential, commercial and industrial uses in the vicinity of the interchange. By improving operational capacity and overall traffic operations, the proposed interchange improvements are anticipated to maintain viable access to the major transportation facilities and employment centers of Southeast Florida (including connectors to freight activity centers/local distribution facilities or between the regional freight corridors). While potential temporary impacts to businesses may occur during project construction as a result of alterations to vehicular access, overall economic enhancements are expected since the improvements are consistent with economic development efforts of the area.

Additional Comments (optional):**CLC Commitments and Recommendations:**

Degree of Effect: 0 None assigned 11/13/2012 by Jeannette Hallock-Solomon, FL Department of Economic Opportunity

Coordination Document: No Involvement

Direct Effects

Identified Resources and Level of Importance:

City of Palm Beach Gardens Comprehensive Plan posted on the City's website, originally adopted in 2009, and most recently amended March, 2011

Comments on Effects to Resources:

Potential to attract new development

The proposed project has potential to attract new development by adding vehicular capacity to I-95 within the affected segments. The project description states that the project's improvements will be critical in supporting the growing bioscience industry in this area; by increasing accessibility to sites targeted by the City for bioscience research development, this project could help to attract new development to the area and new jobs.

Potential to generate jobs

The proposed project has potential to generate jobs through: 1) the construction phase; 2) by providing greater capacity for freight capacity on this important SIS facility linking south Florida with the entirety of the eastern seaboard; and by potentially improving cross-modal accessibility, via PGA Boulevard to rail service (along the FEC RR).

The proposed project is not located in a Rural Area of Critical Economic Concern.

Additional Comments (optional):

CLC Commitments and Recommendations:

The following organization(s) were expected to but did not submit a review of the Economic issue for this alternative: Federal Highway Administration

Land Use

Project Effects

Coordinator Summary Degree of Effect: 2 Minimal assigned 01/14/2013 by FDOT District 4

Comments:

FDEO commented that the project is compatible with the community's development goals as it is depicted on the Future Transportation Map of the City of Palm Beach Gardens Comprehensive Plan, Palm Beach County's Future 2020 Roadway System and Major Parking Facilities Map, and is listed in the Palm Beach Metropolitan Planning Organization (MPO) Transportation Improvement Program. The project is anticipated to support the growing bioscience industry area east of the interchange (which has been designated as part of the Bioscience Research Protection Overlay), as well as the expanding area residential, commercial and industrial uses through enhanced access to major transportation facilities. Since the project supports the land use vision depicted through the City of Palm Beach Gardens Comprehensive Plan, effects on the area's character resulting from the project are anticipated to be minor. Therefore, a Summary DOE of Minimal has been assigned to the Land Use issue.

During Project Development, public outreach will be conducted by FDOT District Four in coordination with the Palm Beach MPO and the City of Palm Beach Gardens to obtain feedback from residents and businesses that may be impacted by the interchange improvements. As the project advances into Project Development, FDOT District Four will coordinate with the City of Palm Beach Gardens, Palm Beach County, and the Palm Beach MPO to ensure that 1) the project is included on the Future Transportation Map of each adopted Comprehensive Plan and is consistent with the adopted Palm Beach MPO Long Range Transportation Plan (LRTP) and 2) funding is identified for all future project phases in the TIP, LRTP, State Transportation Improvement Program (STIP), and FDOT SIS Funding Plan.

Degree of Effect: 2 Minimal assigned 11/19/2012 by Jorge Padron, FDOT District 4

Coordination Document: To Be Determined: Further Coordination Required

Direct Effects

Identified Resources and Level of Importance:

City of Palm Beach Gardens Comprehensive Plan

Palm Beach County Comprehensive Plan

200-Foot Buffer:

2010 Census Designated Places (1)

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]

- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (10)

2008 SFWMD FL Land Use and Land Cover / Acres / Percent

- 1210 FIXED SINGLE FAMILY UNITS / 4.0 / 1.03%

- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 31.5 / 8.17%
- 1400 COMMERCIAL AND SERVICES / 35.8 / 9.28%
- 1411 SHOPPING CENTERS / 16.6 / 4.30%
- 1490 COMMERCIAL AND SERVICES UNDER CONSTRUCTION / 8.2 / 2.12%
- 1820 GOLF COURSE / 5.7 / 1.48%
- 3210 PALMETTO PRAIRIES / 7.7 / 2.00%
- 4110 PINE FLATWOODS / 100.6 / 26.07%
- 4340 UPLAND MIXED CONIFEROUS - HARDWOOD / 1.3 / 0.33%
- 5120 CHANNELIZED WATERWAYS - CANALS / 1.0 / 0.25%
- 5300 RESERVOIRS / 2.9 / 0.75%
- 8140 ROADS AND HIGHWAYS / 170.6 / 44.22%

Future Land Use 2008 / Acres / Percent

- COMMERCIAL, OFFICE, TOURISM, MARINA / 61.2 / 15.87%
- INDUSTRIAL, EXTRACTIVE, TRANSPORTATION / 27.9 / 7.23%
- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 34.7 / 8.99%
- RECREATION/OPEN SPACE / 0.5 / 0.13%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 59.9 / 15.52%
- UNKNOWN, INFORMATION NOT AVAILABLE / 0.0 / 0.01%

500-Foot Buffer:

2010 Census Designated Places (1)

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]
- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (11)

2008 SFWMD FL Land Use and Land Cover / Acres / Percent

- 1210 FIXED SINGLE FAMILY UNITS / 23.6 / 3.41%
- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 78.6 / 11.38%
- 1340 MULTIPLE DWELLING UNITS - HIGH RISE / 2.1 / 0.31%
- 1390 HIGH DENSITY UNDER CONSTRUCTION / 0.4 / 0.06%
- 1400 COMMERCIAL AND SERVICES / 79.0 / 11.44%
- 1411 SHOPPING CENTERS / 22.7 / 3.29%
- 1490 COMMERCIAL AND SERVICES UNDER CONSTRUCTION / 19.5 / 2.82%
- 1700 INSTITUTIONAL / 1.8 / 0.27%
- 1820 GOLF COURSE / 35.6 / 5.15%
- 1850 PARKS AND ZOOS / 4.1 / 0.60%
- 3210 PALMETTO PRAIRIES / 21.2 / 3.07%
- 4110 PINE FLATWOODS / 194.3 / 28.13%
- 4340 UPLAND MIXED CONIFEROUS - HARDWOOD / 3.8 / 0.55%
- 5120 CHANNELIZED WATERWAYS - CANALS / 2.8 / 0.40%
- 5200 LAKES / 0.2 / 0.03%
- 5300 RESERVOIRS / 13.2 / 1.91%
- 8140 ROADS AND HIGHWAYS / 188.0 / 27.20%

Future Land Use 2008 / Acres / Percent

- COMMERCIAL, OFFICE, TOURISM, MARINA / 109.1 / 15.79%
- INDUSTRIAL, EXTRACTIVE, TRANSPORTATION / 60.5 / 8.76%
- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 89.2 / 12.91%
- PUBLIC/SEMI-PUBLIC, GOV, INSTITUTIONAL / 2.4 / 0.35%
- RECREATION/OPEN SPACE / 4.9 / 0.71%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 185.2 / 26.80%
- UNKNOWN, INFORMATION NOT AVAILABLE / 0.1 / 0.01%

1,320-Foot (Quarter-Mile) Buffer:

2010 Census Designated Places (2)

- PALM BEACH GARDENS
- CABANA COLONY

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]
- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (16)

2008 SFWMD FL Land Use and Land Cover / Acres / Percent

- 1210 FIXED SINGLE FAMILY UNITS / 123.5 / 8.04%
- 1290 MEDIUM DENSITY UNDER CONSTRUCTION / 28.4 / 1.85%
- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 186.3 / 12.13%
- 1340 MULTIPLE DWELLING UNITS - HIGH RISE / 30.7 / 2.00%
- 1390 HIGH DENSITY UNDER CONSTRUCTION / 18.7 / 1.22%
- 1400 COMMERCIAL AND SERVICES / 212.0 / 13.80%
- 1411 SHOPPING CENTERS / 23.5 / 1.53%
- 1490 COMMERCIAL AND SERVICES UNDER CONSTRUCTION / 42.8 / 2.79%
- 1550 OTHER LIGHT INDUSTRY / 6.3 / 0.41%
- 1700 INSTITUTIONAL / 7.5 / 0.49%
- 1710 EDUCATIONAL FACILITIES / 0.9 / 0.06%
- 1820 GOLF COURSE / 84.8 / 5.52%
- 1850 PARKS AND ZOOS / 41.2 / 2.68
- 1900 OPEN LAND / 3.7 / 0.24%
- 2110 IMPROVED PASTURES / 14.8 / 0.96%
- 3210 PALMETTO PRAIRIES / 30.0 / 1.95%
- 4110 PINE FLATWOODS / 355.9 / 23.17%
- 4340 UPLAND MIXED CONIFEROUS - HARDWOOD / 18.5 / 1.21%
- 5120 CHANNELIZED WATERWAYS - CANALS / 7.3 / 0.48%
- 5200 LAKES / 2.3 / 0.15%
- 5300 RESERVOIRS / 62.3 / 4.06%
- 8140 ROADS AND HIGHWAYS / 234.8 / 15.28%

Future Land Use 2008 / Acres / Percent

- COMMERCIAL, OFFICE, TOURISM, MARINA / 177.1 / 11.53%
- INDUSTRIAL, EXTRACTIVE, TRANSPORTATION / 148.1 / 9.64%
- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 219.2 / 14.27%
- PUBLIC/SEMI-PUBLIC, GOV, INSTITUTIONAL / 16.0 / 1.04%
- RECREATION/OPEN SPACE / 30.2 / 1.96%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 601.7 / 39.17%
- UNKNOWN, INFORMATION NOT AVAILABLE / 0.2 / 0.01%

Comments on Effects to Resources:

The area surrounding the existing I-95 and PGA Boulevard interchange is urbanized containing a mixture of commercial, industrial, mixed-use and residential land uses with vacant land in the northeast quadrant. According to the City of Palm Beach Gardens Comprehensive Plan, future land use is to remain relatively unchanged, with the exception of the area east of the interchange which has been designated as part of the *Bioscience Research Protection Overlay* (BRPO). The BRPO was developed to protect portions of land for biotechnology/biosciences land uses and includes the Scripps Florida Phase II/Briger Tract DRI which consists of 82 acres located south of Donald Ross Road, north of Hood Road and east and west of I-95 (just north of the study area). The DRI includes 1,600,000 square feet of Biotech Research and Development, 2,400,000 square feet of biotechnological/biomedical, pharmaceutical, and office space, 2,700 residential dwelling units, and 500,000 square feet of retail space. One additional Development of Regional Impact and several Planned Unit Developments, primarily composing portions of the BRPO, are also concentrated in the project area. While exact right-of-way requirements are unknown at this time, effects on the areas character resulting from the interchange improvements are anticipated to be minimal as the project is expected to support the growing bioscience industry and vision of the City and County, as well as the expanding residential, commercial and industrial uses in the vicinity of the interchange.

Transportation Plan Consistency:

Funding for the project PD&E Study [including the Interchange Modification Report (IMR)/Interchange Justification Report (IJR)] is programmed in the FDOT Work Program (FM #413265-1) and the *SIS Funding Strategy First Five-Year Plan*. In addition, the project is included in the FY 2012/2013 2016/2017 Transportation Improvement Program (TIP) of the Palm Beach Metropolitan Planning Organization (MPO). While the interchange improvements at PGA Boulevard are not included in the Cost-Feasible component of the Palm Beach MPO 2035 Long Range Transportation Plan (LRTP), several projects in the study area are identified (including the provision of a new interchange at I-95 and Central Boulevard).

Additional Comments (optional):**CLC Commitments and Recommendations:**

Degree of Effect: 0 None assigned 11/13/2012 by Jeannette Hallock-Solomon, FL Department of Economic Opportunity

Coordination Document: No Involvement

Direct Effects**Identified Resources and Level of Importance:**

City of Palm Beach Gardens Comprehensive Plan posted on the City's website, originally adopted in 2009, and most recently amended March, 2011

Comments on Effects to Resources:

The project, as described, appears to be compatible with the communitys development goals, as articulated in the City of Palm Beach Gardenss comprehensive plan. The project will likely add capacity to the surface transportation system and further mobility goals and objectives.

Future Land Use Categories

The proposed project is surrounded by the following land use categories: RH-Residential High; I-Industrial; C- Commercial; ROS-Recreation Open Space; RM-Residential Medium; MXD-Mixed Use; PO-Professional Office; RL-Rural Low; and, the BioScience Research Protection Overlay.

There are no conservation preserved lands nor any lands designated *conservation* on the Citys future land use map located within, adjacent or near to the ETDM Alternative Polygon.

Future Transportation Map

In accordance with its comprehensive plan, the City of Palm Beach Gardens is required to coordinate transportation planning with Palm Beach County and the Palm Beach MPO. The project is depicted on the Countys Future 2020 Roadway System and Major Parking Facilities Map and is listed in the MPOs TIP (Transportation Improvement Program).

Local Parks

The proposed project is within a quarter mile of several City of Palm Beach Gardens parks. The project is adjacent to the City Park, which is itself adjacent to Palm Beach Gardens Tennis Center; in combination, the two facilities total approximately 31.75 acres. The project is also adjacent to Gardens Park Soccer Facility, which is itself adjacent to Gardens Park Baseball Facility; another park, Burns Road Recreational Center is located across the street from these parks but slightly beyond a quarter mile from the project. The combined size of the baseball and soccer facilities is approximately 36.3 acres. The impacts to these potential 4(f) resources should be analyzed if the proposed project moves forward.

Miscellaneous Items

The FEC railroad right-of-way and tracks, located within and adjacent to the ETDM Alternative Polygon, provides an opportunity to facilitate enhanced cross-modal and multimodal transportation options; contemplated ETDM projects could be designed to facilitate this strategy, as well the potential location of a future train station, and in general, transit-oriented development in the area near the intersection of PGA Boulevard and Alternate A-1-A.

The following objective and policy from the future land use element of the Citys Comprehensive Plan reference this opportunity: Objective 1.3.5.: Adopt land development regulations for the purpose of promoting the location of a rail station in the City and providing sustainable development that encourages multi-modal transit.

Policy 1.3.5.2.: Within two years after it has been determined that the Tri-Rail will utilize the FEC tracks and serve northeastern Palm Beach County , the City shall undertake a study of the vacant lands lying within one quarter mile of the FEC railroad line paralleling Alternate A1A. The purpose of this study is to determine the location(s) best suited for a future train station and appropriate uses for vacant lands in order to promote transit-oriented development. The study shall consider desired forms of development patterns outlined in the Treasure Coast Strategic Regional Policy Plan.

The Regional Center DRI, featuring a 1.6 million square foot indoor shopping mall surrounded by 750,000 square feet of professional offices and similar service businesses and 1645 residential units, is located adjacent to the ETDM Alternative Polygon. The DRI is adjacent to and accessible by both PGA Boulevard and Alternate A-1-A, and is immediately proximate to the FEC RR. The project has the potential to increase accessibility to this DRI via vehicular travel, particularly for regional and extra-regional travelers; if designed appropriately, the project also offers potential to enhance multimodal accessibility.

Some tracts of vacant land remain within the immediate vicinity of the ETDM Alternative Polygon; additional traffic volume resulting from any additional capacity provided through the project could enhance the development potential and economic potential of these tracts.

A significant portion of the area within and adjacent to the ETDM Alternative Polygon lies within the BioScience Research Protection Overlay, as designated on the City of Palm Beach Gardens future land use map. As articulated in the future land use element of the Citys Comprehensive Plan, this overlay is intended for the purpose of promoting the bioscience industry.

The Plan contemplates transit-supportive development, a pedestrian-friendly environment, and an interconnected transportation network. The Citys Comprehensive Plan calls for PGA Boulevard, included as an ETDM Alternative Segment, to be developed as a main street, with techniques to ensure sustainability, including the utilization of landscaping, boulevard strips, pedestrian walkways, bikeways, buffers, and setbacks to emphasize the various functions of PGA Boulevard as a divider of different land uses and as a center of the City. Policy 1.3.1.4. of the future land use element provides further detail:

Policy 1.3.1.4.:PGA Boulevard shall be developed as the "Main Street" of Palm Beach Gardens using the following techniques to ensure sustainability:

- a. Following completion of the PGA Boulevard/Alternate A1A urban interchange, a new CRALLS (Constrained Roadway at a Lower Level of Service) Level of Service Standard for PGA Boulevard shall be determined in coordination with Palm Beach County, the Regional Planning Council, and the State Department of Transportation with the maximum number of lanes being six;
- b. The City shall maintain the PGA design guidelines as regulations which require the utilization of landscaping, boulevard strips, pedestrian walkways, bikeways, buffers, and setbacks to emphasize the various functions of PGA Boulevard as a divider of different land uses and as a center of the City.

The proposed project is not located in an Area of Critical State Concern, not located in a Coastal High Hazard Area, and is not located

near a military base.

Additional Comments (optional):

CLC Commitments and Recommendations:

The following organization(s) were expected to but did not submit a review of the Land Use issue for this alternative: Federal Highway Administration

Mobility

Project Effects

Coordinator Summary Degree of Effect: 1 *Enhanced* assigned 01/14/2013 by FDOT District 4

Comments:

The proposed interchange improvements are anticipated to 1) achieve acceptable Levels of Service (LOS) at the interchange by accommodating future travel demand; 2) allow I-95 to continue to facilitate the north-south movement of traffic in Southeast Florida; 3) enhance freight mobility by maintaining viable access to major transportation facilities and businesses (i.e., freight activity centers/local distribution facilities); and 4) improve emergency evacuation and response times. Therefore, a Summary DOE of Enhanced has been assigned to the Mobility issue.

During Project Development, public outreach will be conducted by FDOT District Four in coordination with the Palm Beach Metropolitan Planning Organization and the City of Palm Beach Gardens to solicit community opinions and preferences (targeting input from the transportation disadvantaged population) regarding the project.

Degree of Effect: 1 *Enhanced* assigned 11/19/2012 by Jorge Padron, FDOT District 4

Coordination Document: To Be Determined: Further Coordination Required

Direct Effects

Identified Resources and Level of Importance:

200-Foot Buffer:

FDOT RCI Bridges (10)

Bus Transit Routes (5)

Facility Crossings (15)

Greenways Ecological Priority Linkages (Low 99.3 Acres)

Railways

- RAILROAD: 3,229.66 Linear Feet

500-Foot Buffer:

FDOT RCI Bridges (10)

Bus Transit Routes (5)

Facility Crossings (15)

Greenways Ecological Priority Linkages (Low 185.5 Acres)

Railways

- RAILROAD: 3,843.39 Linear Feet

1,320-Foot (Quarter-Mile) Buffer:

FDOT RCI Bridges (13)

Noise Barriers (5)

Bus Transit Routes (5)

Facility Crossings (18)

Greenways Ecological Priority Linkages (Low 403.3 Acres)

Railways

- RAILROAD: 5,488.97 Linear Feet

Number of Housing Units with No Vehicle Available: 400 (5.4%)

Comments on Effects to Resources:

By improving operational capacity and overall traffic operations, the proposed interchange improvements are anticipated to 1) achieve acceptable Levels of Service (LOS) at the interchange in the future condition by accommodating future travel demand projected as a result of Palm Beach County population and employment growth; 2) allow I-95 to continue to serve as a critical arterial in facilitating the north-south movement of traffic in Southeast Florida as it connects major employment centers, residential areas, and other regional destinations between Miami-Dade, Broward and Palm Beach Counties; and 3) enhance freight mobility by maintaining viable access to the major transportation facilities and businesses of the area (including connectors to freight activity

centers/local distribution facilities or between the regional freight corridors).

Further, as both I-95 and PGA Boulevard serve as part of the emergency evacuation route network designated by the Florida Division of Emergency Management, the proposed project is anticipated to enhance emergency evacuation and response times by 1) improving connectivity and accessibility to I-95 and other major arterials designated on the state evacuation route network and 2) increasing the number of residents that can be evacuated during an emergency event through expanded operational capacity. While potential temporary impacts to residences and businesses may occur during project construction as a result of alterations to vehicular access, overall mobility enhancements are expected.

Additional Comments (optional):

CLC Commitments and Recommendations:

The following organization(s) were expected to but did not submit a review of the Mobility issue for this alternative: Federal Highway Administration

Relocation

Project Effects

Coordinator Summary Degree of Effect: 3 *Moderate* assigned 01/14/2013 by FDOT District 4

Comments:

While the majority of the project traverses an urbanized area, relocation impacts are anticipated to be low since the majority of houses and businesses are set back from the road/interchange. However, due to FHWA concerns regarding potential impacts to commercial properties surrounding the interchange at I-95 and PGA Boulevard, a Summary DOE of Moderate has been assigned to the Relocation issue.

Potential relocation effects will be assessed further during Project Development as more detailed and finalized project information regarding right-of-way needs becomes available. The proposed interchange improvements and any roadway typical sections will be adjusted so as to avoid or minimize impacts to identified features.

Degree of Effect: 3 *Moderate* assigned 11/19/2012 by Linda Anderson, Federal Highway Administration

Coordination Document: PD&E Support Document As Per PD&E Manual

Direct Effects

Identified Resources and Level of Importance:

Within 100' buffer:

1.5 acres of Fixed Single Family Units.

16.4 acres of Low Rise Dwelling Units.

Within 200' buffer:

4.0 acres of Fixed Single Family Units.

31.5 acres of Low Rise Dwelling Units.

Adjacent to the PGA Blvd./I-95 interchange, for which changes are proposed, lies the Double Tree Hotel, Embassy Suites, Classical Pilates, and the Florida School for Dance Education.

Comments on Effects to Resources:

It appears unlikely that residential relocations will be required as the residential areas cluster around the I-95/Military Trail intersection for which removal of ramp connections are proposed.

Regarding the PGA Blvd./I-95 interchange, it appears that it would be difficult to expand this interchange without impacting the commercial properties described above.

Additional Comments (optional):

CLC Commitments and Recommendations:

Degree of Effect: 2 *Minimal* assigned 11/19/2012 by Jorge Padron, FDOT District 4

Coordination Document: To Be Determined: Further Coordination Required

Direct Effects

Identified Resources and Level of Importance:

200-Foot Buffer:

2010 Census Designated Places (1)

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]
- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (10)**Geocoded Homeowner and Condominium Associations (1)**

- TRAILS END VILLAS

Cultural Field Survey Areas (5)**Resource Groups (3)****FDOT RCI Bridges (10)****Facility Crossings (15)****Greenways Ecological Priority Linkages (Low 99.3 Acres)****SFWMD Residential Areas 2008**

- 1210 FIXED SINGLE FAMILY UNITS / 4.0 / 1.03%
- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 31.5 / 8.17%

Residential Areas - Future Land Use 2008 / Acres / Percent

- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 34.7 / 8.99%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 59.9 / 15.52%

500-Foot Buffer:**2010 Census Designated Places (1)**

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]
- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (11)**Geocoded Homeowner and Condominium Associations (1)**

- TRAILS END VILLAS

Cultural Field Survey Areas (6)**Resource Groups (3)****FDOT RCI Bridges (10)****Facility Crossings (15)****Greenways Ecological Priority Linkages (Low 185.5 Acres)****SFWMD Residential Areas 2008**

- 1210 FIXED SINGLE FAMILY UNITS / 23.6 / 3.41%
- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 78.6 / 11.38%
- 1340 MULTIPLE DWELLING UNITS - HIGH RISE / 2.1 / 0.31%
- 1390 HIGH DENSITY UNDER CONSTRUCTION / 0.4 / 0.06%

Residential Areas - Future Land Use 2008 / Acres / Percent

- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 89.2 / 12.91%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 185.2 / 26.80%

Comments on Effects to Resources:

The interchange at I-95 and PGA Boulevard is a mixed layout with a diamond configuration for the southbound terminal, a loop ramp for the northbound to westbound transition, and a direct connect flyover ramp for the westbound to southbound I-95 movement. I-95 is currently a ten-lane divided facility (eight general use and two HOV lanes) with a functional classification of '*Urban Principal Arterial Interstate*'. The existing right-of-way varies as it approaches the interchange, but the typical right-of-way ranges from approximately 324 to 400 feet. PGA Boulevard is a six-lane divided east-west facility with a functional classification of *Urban Principal Arterial*. The existing right-of-way varies from approximately 120 to 160 feet west of I-95 and is typically 114 feet east of I-95. The ultimate interchange improvements could potentially require additional right-of-way; however, the specific right-of-way requirements are not known at this time and will be determined through further analysis.

While the interchange is located in an urbanized area, relocation impacts are anticipated to be minimal since the majority of houses and businesses are set back from the facility.

Additional Comments (optional):**CLC Commitments and Recommendations:**

Social

Project Effects

Coordinator Summary Degree of Effect: 3 *Moderate* assigned 01/14/2013 by FDOT District 4

Comments:

FHWA noted that households living below the poverty line are located within the census block groups overlapping the 100-foot project buffer area; the demographics of these block groups are as follows 1.2 - 19% African American, 7.7 - 20.4% Hispanic/Latino, 1.8-6.9% Asian, 0.1-0.4% American Indian/Alaska Native, and 0.2-4.9% "Other Race". While potential noise and vibration effects may be of concern to proximate residences and businesses, the overall impacts on community character and social cohesion are anticipated to be minimal as the project is expected to support the growing bioscience industry and vision of the City and County, as well as the expanding residential, commercial and industrial uses in the vicinity of the interchange. However, due to FHWA concerns regarding potential project effects (particularly to low income/minority populations), a Summary DOE of Moderate has been assigned to the Social issue.

During the Project Development phase, public outreach will be conducted by FDOT District Four in coordination with the Palm Beach Metropolitan Planning Organization and the City of Palm Beach Gardens to solicit input from the general public to ensure that both the social and transportation needs of the community are addressed through the project. To avoid and/or minimize potential impacts to the greatest extent practicable, FDOT District Four will also prepare an Air Quality Technical Memorandum, Noise Study Report, and Sociocultural Effects Evaluation.

Degree of Effect: 0 *None* assigned 11/28/2012 by Maher Budeir, US Environmental Protection Agency

Coordination Document: PD&E Support Document As Per PD&E Manual

Direct Effects

Identified Resources and Level of Importance:

Comments on Effects to Resources:

Additional Comments (optional):

CLC Commitments and Recommendations:

Degree of Effect: 3 *Moderate* assigned 11/19/2012 by Linda Anderson, Federal Highway Administration

Coordination Document: PD&E Support Document As Per PD&E Manual

Direct Effects

Identified Resources and Level of Importance:

According to the 2010 American Community Survey Block Group Data, within the block groups overlapping the 100'project buffer, the median family income ranges from \$35,938 to \$121,364. Households living below the poverty line range from 9-120, and households receiving public assistance range from 0-8, depending on the blockgroup.

According to the 2010 US Census Block Group Data, the population living within the blockgroups overlapping the 100' project buffer is 1.2 - 19% African American, 7.7 - 20.4% Hispanic/Latino, 1.8-6.9% Asian, 0.1-0.4% American Indian/Alaska Native, and 0.2-4.9% "other race."

Comments on Effects to Resources:

The law prohibits disproportionate impacts to individuals with low incomes and/or who belong to protected classes. The data described above indicates that such individuals are present adjacent to the project APE.

A socio-cultural effects study is required to determine whether the project will have environmental justice impacts.

Additional Comments (optional):

CLC Commitments and Recommendations:

Degree of Effect: 2 *Minimal* assigned 11/19/2012 by Jorge Padron, FDOT District 4

Coordination Document: To Be Determined: Further Coordination Required

Direct Effects

Identified Resources and Level of Importance:

200-Foot Buffer:

2010 Census Designated Places (1)

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]

- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (10)

Geocoded Civic Centers (1)

- DOUBLETREE HOTEL PALM BEACH GARDENS

Geocoded Community Centers (1)

- PALM BEACH GARDENS LIONS CLUB

Geocoded Cultural Centers (1)

- ONESSIMO FINE ART (PALM BEACH GARDENS)

Geocoded Government Buildings (1)

- CITY OF PALM BEACH GARDENS CITY HALL

Geocoded Homeowner and Condominium Associations (1)

- TRAILS END VILLAS

Geocoded Laser Facilities (1)

- DERMA SILKE, LLC

Geocoded Social Service Facilities (3)

- MARINO DOMINICK A OD PA
- DIVORCE MEDIATION SPECIALISTS
- GENTLEMEN OF THE GARDEN, INC.

Cultural Field Survey Areas (5)

Resource Groups (3)

FDOT RCI Bridges (10)

Noise Barriers (5)

Bus Transit Routes (5)

Facility Crossings (15)

Greenways Ecological Priority Linkages (Low 99.3 Acres)

Railways

- RAILROAD: 3,229.66 Linear Feet

500-Foot Buffer:

2010 Census Designated Places (1)

- PALM BEACH GARDENS

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]
- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (11)

Geocoded Civic Centers (1)

- DOUBLETREE HOTEL PALM BEACH GARDENS

Geocoded Community Centers (2)

- PALM BEACH GARDENS LIONS CLUB
- MCCARTEN ASSOCIATES

Geocoded Cultural Centers (2)

- ONESSIMO FINE ART (PALM BEACH GARDENS)
- STUDIO E GALLERY (PALM BEACH GARDENS)

Geocoded Government Buildings (1)

- CITY OF PALM BEACH GARDENS CITY HALL

Geocoded Homeowner and Condominium Associations (1)

- TRAILS END VILLAS

Geocoded Laser Facilities (2)

- DERMA SILKE, LLC
- LASER & EYE SURGERY CENTER OF PALM BEACH

Geocoded Social Service Facilities (3)

- MARINO DOMINICK A OD PA
- DIVORCE MEDIATION SPECIALISTS
- GENTLEMEN OF THE GARDEN, INC.

Cultural Field Survey Areas (6)

Resource Groups (3)

FDOT RCI Bridges (10)

Noise Barriers (5)

Bus Transit Routes (5)

Facility Crossings (15)

Greenways Ecological Priority Linkages (Low 185.5 Acres)

Railways

- RAILROAD: 3,843.39 Linear Feet

1,320-Foot (Quarter-Mile) Buffer:**2010 Census Designated Places (2)**

- PALM BEACH GARDENS
- CABANA COLONY

Developments of Regional Impact (2)

- NORTHCORP CENTER [ADA NO: 1989-011]
- THE REGIONAL CENTER [ADA NO: 1982-036]

Planned Unit Development (16)**FDEM Law Enforcement (1)**

- PALM BEACH GARDENS POLICE DEPARTMENT

Geocoded Civic Centers (3)

- DOUBLETREE HOTEL PALM BEACH GARDENS
- MARRIOTT PALM BEACH GARDENS
- BONNETTE BANQUET & LODGE

Geocoded Community Centers (4)

- PALM BEACH GARDENS LIONS CLUB
- MCCARTEN ASSOCIATES
- NORTH PALM BEACH COUNTY CHAMBER OF COMMERCE
- THE CHORAL SOCIETY OF THE PALM BEACHES

Geocoded Cultural Centers (4)

- ONESSIMO FINE ART (PALM BEACH GARDENS)
- STUDIO E GALLERY (PALM BEACH GARDENS)
- GARDENS CINAMAX MOVIE THEATRES
- CHABAD OF PALM BEACH GARDENS

Geocoded Government Buildings (1)

- CITY OF PALM BEACH GARDENS CITY HALL

Geocoded Health Care Facilities (1)

- GARDENS COURT NURSING HOME

Geocoded Homeowner and Condominium Associations (3)

- TRAILS END VILLAS
- RESIDENCES AT MIDTOWN
- SABAL RIDGE

Geocoded Laser Facilities (5)

- DERMA SILKE, LLC
- LASER & EYE SURGERY CENTER OF PALM BEACH
- BODY DETAILS, INC.
- RADIANCE MEDSPA
- NEW RADIANCE

Geocoded Parks (3)

- GARDENS PARK
- GARDENS TENNIS CENTER
- ANOTHER GENERATION

Geocoded Religious Centers (3)

- PALM BEACH COUNSELING CENTER
- FIRST BAPTIST CHURCH
- PALM BEACH COMMUNITY CHURCH

Geocoded Social Service Facilities (6)

- MARINO DOMINICK A OD PA
- DIVORCE MEDIATION SPECIALISTS
- GENTLEMEN OF THE GARDEN, INC.
- PALM BEACH COUNSELING CENTER
- COMFORT KEEPERS
- THE GARDENS COURT

Group Care Facilities (2)

- CHESTERBROOK ACADEMY
- KAMAE HALTAUFDERHYDE

Cultural Field Survey Areas (8)

Resource Groups (3)

National Historic Preservation Act Florida Certified Local Governments (1)

- PALM BEACH COUNTY

FDOT RCI Bridges (13)

Noise Barriers (5)

Bus Transit Routes (5)

Facility Crossings (18)

Greenways Ecological Priority Linkages (Low 403.3 Acres)

Railways

- RAILROAD: 5,488.97 Linear Feet

2008 SFWMD FL Land Use and Land Cover / Acres / Percent

- 1210 FIXED SINGLE FAMILY UNITS / 123.5 / 8.04%
- 1290 MEDIUM DENSITY UNDER CONSTRUCTION / 28.4 / 1.85%
- 1330 MULTIPLE DWELLING UNITS - LOW RISE / 186.3 / 12.13%
- 1340 MULTIPLE DWELLING UNITS - HIGH RISE / 30.7 / 2.00%
- 1390 HIGH DENSITY UNDER CONSTRUCTION / 18.7 / 1.22%
- 1400 COMMERCIAL AND SERVICES / 212.0 / 13.80%
- 1411 SHOPPING CENTERS / 23.5 / 1.53%
- 1490 COMMERCIAL AND SERVICES UNDER CONSTRUCTION / 42.8 / 2.79%
- 1550 OTHER LIGHT INDUSTRY / 6.3 / 0.41%
- 1700 INSTITUTIONAL / 7.5 / 0.49%
- 1710 EDUCATIONAL FACILITIES / 0.9 / 0.06%
- 1820 GOLF COURSE / 84.8 / 5.52%
- 1850 PARKS AND ZOOS / 41.2 / 2.68
- 1900 OPEN LAND / 3.7 / 0.24%
- 2110 IMPROVED PASTURES / 14.8 / 0.96%
- 3210 PALMETTO PRAIRIES / 30.0 / 1.95%
- 4110 PINE FLATWOODS / 355.9 / 23.17%
- 4340 UPLAND MIXED CONIFEROUS - HARDWOOD / 18.5 / 1.21%
- 5120 CHANNELIZED WATERWAYS - CANALS / 7.3 / 0.48%
- 5200 LAKES / 2.3 / 0.15%
- 5300 RESERVOIRS / 62.3 / 4.06%
- 8140 ROADS AND HIGHWAYS / 234.8 / 15.28%

Future Land Use 2008 / Acres / Percent

- COMMERCIAL, OFFICE, TOURISM, MARINA / 177.1 / 11.53%
- INDUSTRIAL, EXTRACTIVE, TRANSPORTATION / 148.1 / 9.64%
- MIXED USE, ACTIVITY CENTER, URBAN VILLAGE / 219.2 / 14.27%
- PUBLIC/SEMI-PUBLIC, GOV, INSTITUTIONAL / 16.0 / 1.04%
- RECREATION/OPEN SPACE / 30.2 / 1.96%
- RESIDENTIAL MEDIUM MORE THAN RL, < 13DU / 601.7 / 39.17%
- UNKNOWN, INFORMATION NOT AVAILABLE / 0.2 / 0.01%

Comments on Effects to Resources:

By improving operational capacity and overall traffic operations, the proposed interchange improvements are anticipated to 1) accommodate the future travel demand projected as a result of Palm Beach County population and employment growth and 2) allow I-95 to continue to serve as a critical arterial in facilitating the north-south movement of traffic in Southeast Florida as it connects major employment centers, residential areas, and other regional destinations between Miami-Dade, Broward and Palm Beach Counties.

The area surrounding the existing I-95 and PGA Boulevard interchange is urbanized containing a mixture of commercial, industrial, mixed-use and residential land uses with vacant land in the northeast quadrant. Community features that occur within the vicinity of the project include: one Census Designated Place (Palm Beach Gardens), one civic center, two community centers, two cultural centers, one government building (City of Palm Beach Gardens City Hall), one homeowner and condominium association, two laser facilities, three social service facilities, five bus transit routes, railway, cultural resources, and recreational features.

According to the City of Palm Beach Gardens Comprehensive Plan, future land use is to remain relatively unchanged, with the exception of the area east of the interchange which has been designated as part of the *Bioscience Research Protection Overlay* (BRPO). The BRPO was developed to protect portions of land for biotechnology/biosciences land uses and includes the Scripps Florida Phase II/Briger Tract DRI which consists of 82 acres located south of Donald Ross Road, north of Hood Road and east and west of I-95 (just north of the study area). The DRI includes 1,600,000 square feet of Biotech Research and Development, 2,400,000 square feet of biotechnological/biomedical, pharmaceutical, and office space, 2,700 residential dwelling units, and 500,000 square feet of retail space. One additional Development of Regional Impact and several Planned Unit Developments, primarily composing portions

of the BRPO, are also concentrated in the project area.

The table below presents the demographic data for both the 1,320-foot (quarter-mile) project buffer and Palm Beach County. According to the EST GIS analysis results, the demographic profile of the buffer area differs from the profile of Palm Beach County as a whole, with fewer minority populations. The White population of the buffer area is 16.6% higher than the estimate for Palm Beach County, while the African-American population is 11.9% lower. The Hispanic population of the buffer area is also 6.1% less than the Hispanic population of the overall county. While the buffer area contains slightly lower elderly (persons of age 65 or above) and youth (individuals under age 18) populations compared to the county, the buffer area population is wealthier and has greater access to transportation compared to the Palm Beach County population as a whole.

Demographic / 1,320-Foot Buffer / Palm Beach County

White (Race)* / 90.1% / 73.5%

African-American (Race)* / 5.4% / 17.3%

"Other" *** (Race)* / 4.5% / 6.9%

Hispanic (Ethnic Group)* / 12.9% / 19.0%

Age 65+ ** / 20.4% / 21.1%

Under Age 18 ** / 17.1% / 21.9%

Housing Units with No Vehicle Available ** / 5.4% / 6.2%

Averaged Median Family Income ** / \$68,704 / \$53,242

* Source: US Census Bureau (2010 US Census)

** Source: US Census Bureau (2010 American Community Survey)

*** "Other" includes Asian, Native American, Native Hawaiian & Other Pacific Islander Alone, & Other Race.

It should be noted that 8 census blocks within the 1,320-foot project buffer contain a minority population greater than 40%. A total of 793 individuals comprise the minority population of these census blocks. It should additionally be noted that 345 persons within the 1,320-foot buffer (3.8% of the total buffer population) indicated a deficiency in English proficiency. Limited English Proficiency (LEP) accommodations are not necessary at this time as they are only required if the demographic data indicates that 5.0% or 1,000 persons or more in a project area speak a language other than English (per Part 1, Chapter 11, Section 11-1.2.4 of the FDOT PD&E Manual).

The project is expected to support the growing bioscience industry and vision of the City and County, as well as the expanding residential, commercial and industrial uses in the vicinity of the interchange. While potential noise and vibration effects may be of concern to proximate residences and businesses, the impacts on community character and social cohesion as a result of the interchange improvements are anticipated to be minimal. In addition, noise barriers are present to help reduce impacts.

Additional Comments (optional):

CLC Commitments and Recommendations:

ETAT Reviews and Coordinator Summary: Secondary and Cumulative

Secondary and Cumulative Effects

Project Effects

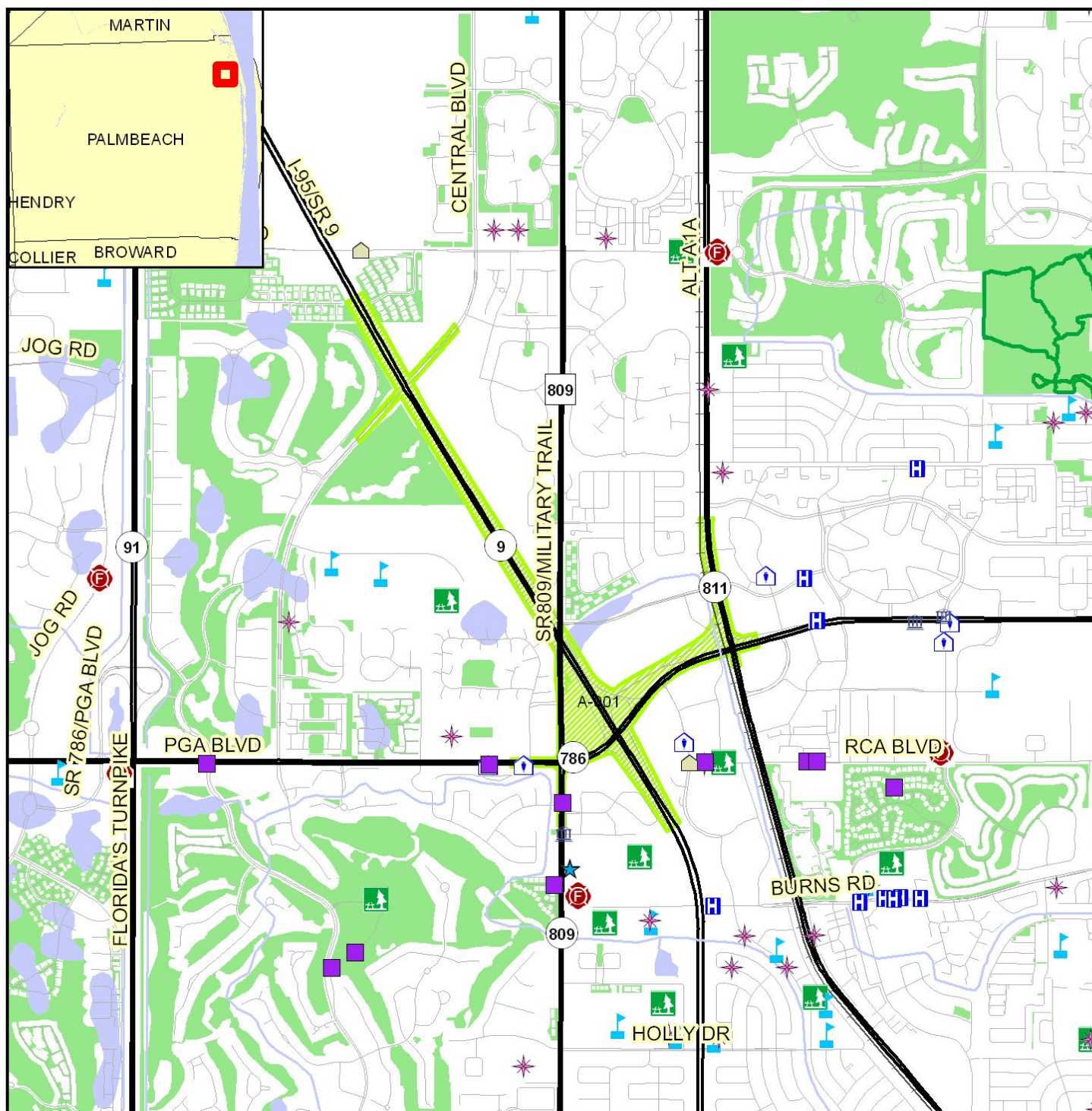
Coordinator Summary Degree of Effect: 2 *Minimal* assigned 01/14/2013 by FDOT District 4

Comments:

No secondary and cumulative effects were identified to date by any agency for this project. Therefore, a Summary DOE of Minimal has been assigned to the Secondary and Cumulative Effects issue due to the fact that the project is anticipated to support the growing bioscience industry area east of the interchange (which has been designated as part of the Bioscience Research Protection Overlay), as well as the expanding area residential, commercial and industrial uses through enhanced access to major transportation facilities. During Project Development, FDOT District Four will coordinate with all appropriate agencies to adequately address any potential direct, indirect, and cumulative project effects on historic and archaeological sites, wildlife and habitat, wetlands, public lands, neighborhoods, Title VI/Environmental Justice populations, and water resources.

None found

13748 Interchange Improvements to SR 9 (I-95) at PGA Boulevard and Central Boulevard



0.00.08 Miles



- | | | | |
|--|---|--|---|
| <ul style="list-style-type: none"> ETDM Alternative Point ETDM Alternative Terminus ETDM Alternative Segment ETDM Alternative Polygon Major Road Local Road or Trail | <ul style="list-style-type: none"> Government Civic Center Cemetery Social Service Community Center Law Enforcement Place of Worship | <ul style="list-style-type: none"> Cultural Center Fire Station Health Care School Park | <ul style="list-style-type: none"> River, Stream or Canal Recreational Trail Railroad Community Boundary Water Body Conservation or Recreation Area |
|--|---|--|---|

Data Sources:
US Geological Survey; FL Department of Transportation; NAVTEQ; FL Property Appraisers; FL Natural Areas Inventory

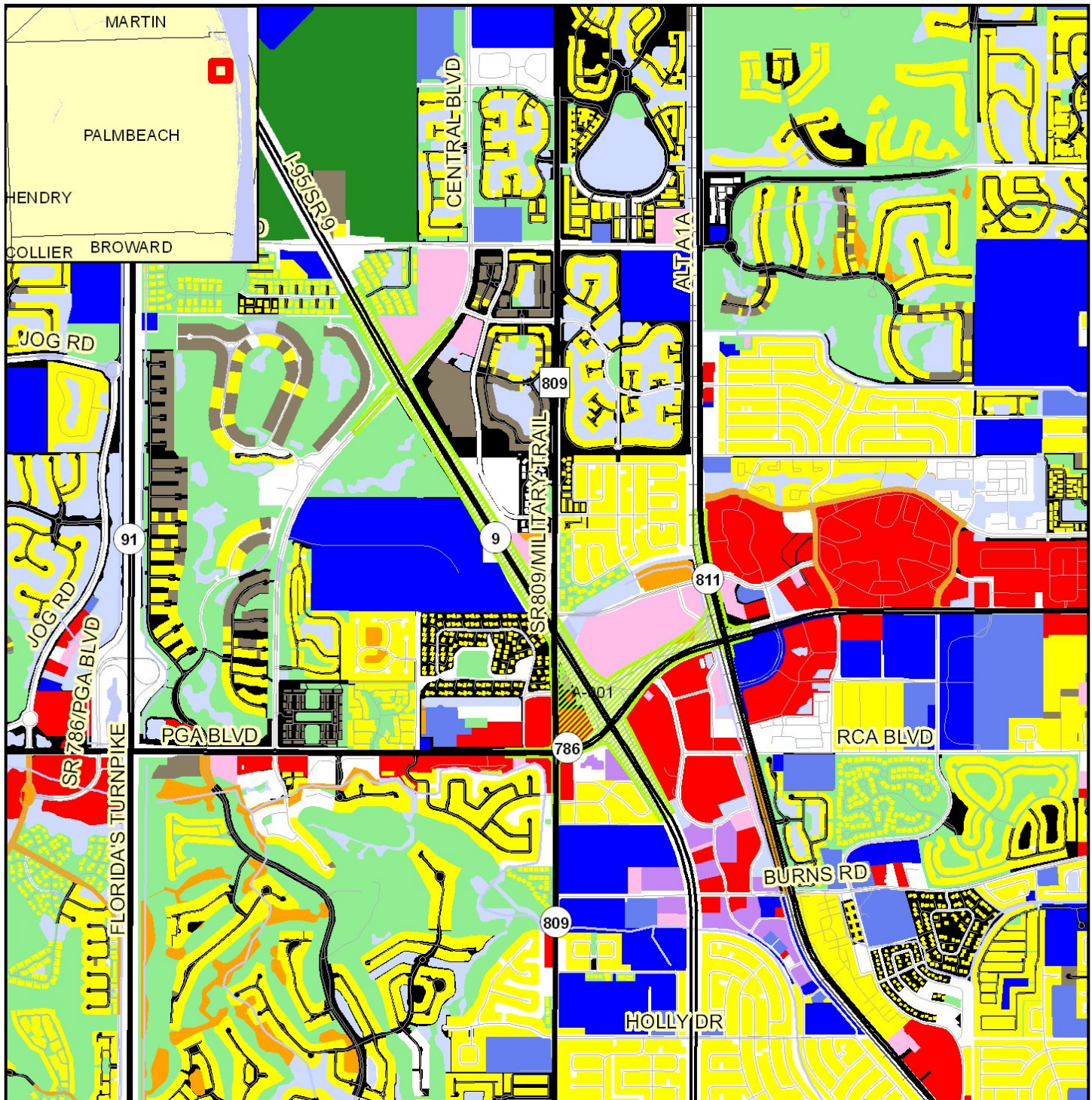
etdm
Efficient Transportation Decision Making

Environmental Screening Tool **est**

Map Generated on: 9/19/2012



13748 Interchange Improvements to SR 9 (I-95) at PGA Boulevard and Central Boulevard



0 0.25 Miles



Data Sources:
NAVTEQ
US Geological Survey
Florida Department of Revenue
Florida Department of Transportation
Florida County Property Appraiser Offices

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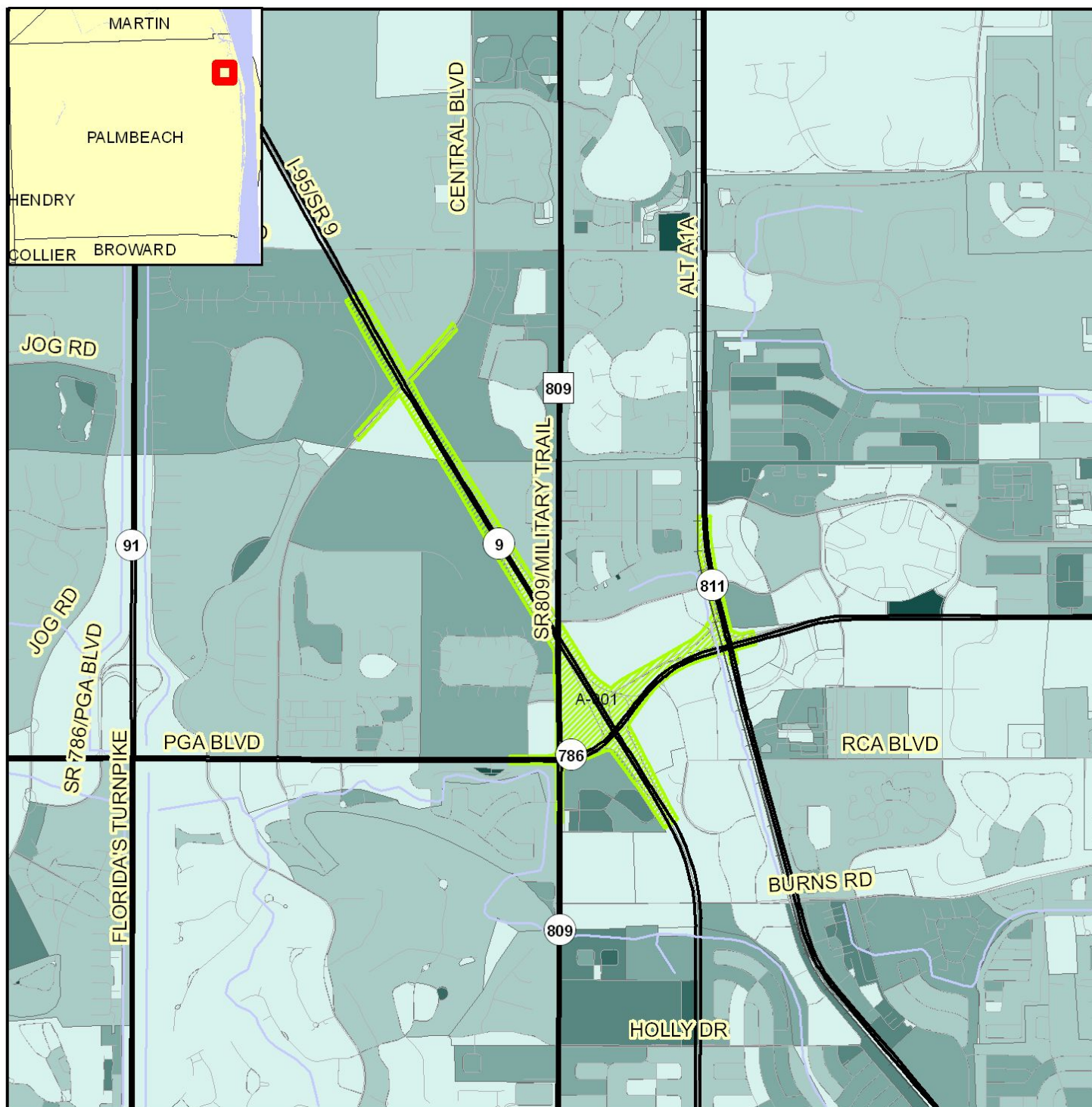
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Map Generated on: 9/19/2012



13748 Interchange Improvements to SR 9 (I-95) at PGA Boulevard and Central Boulevard



Minority Population Distribution Map

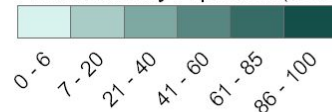
0 0.1 Miles



Data Sources:
US Geological Survey
FL Department of Transportation
NAVTEQ
US Census Bureau (2010)

- ETDM Alternative Point
- ETDM Alternative Terminus
- ETDM Alternative Segment
- ETDM Alternative Polygon
- Major Road
- Local Road or Trail
- Railroad
- River, Stream or Canal
- Water Body

Percent Minority Population (2010)



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